

AGENDA ITEM:
MEETING DATE: July 26, 2016
TO: City Council and Planning Commission
FROM: Brett Limbaugh, Development Services Director
PRESENTER: Bob Paulsen, Current Planning Manager

TITLE: Options for amendments to regulations in Chapter 18.50 on electronic message signs in the I-25 corridor.

RECOMMENDED ACTION: Provide comments to City staff and give direction on how to proceed.

INTRODUCTION:

This memorandum provides options for amending the Loveland Zoning Code relative to the provisions for electronic message sign in the I-25 corridor. These options are intended to address the issues raised at the February 22, 2016 Planning Commission meeting during the discussion on allowances for electronic message signs on property “directly abutting” the I-25 corridor.

BACKGROUND:

1. **Zoning Code:** The zoning code allows one electronic message sign for “commercial centers” on lots “directly abutting” the right-of-way of I-25 for a linear distance of more than 500 feet. The terms “directly abutting” and “commercial centers” are not defined in the code.
2. **Applicant’s Request:** The property owner at the northwest corner of I-25 and Byrd Drive filed an application for an electronic message sign. Code provisions specify that a minimum of 500 linear feet of frontage coinciding with the I-25 right-of-way is required to qualify for an electronic sign. The property line is approximately 370 linear feet. Since a variance was not available to address this situation, the applicant requested that the term “directly abutting” be interpreted to not only include the property line coinciding with the I-25 right-of-way, but also the property line coinciding with the right-of-way of Byrd Drive, which is perpendicular to I-25.
3. **Current Planning Manager’s Interpretation:** The Current Planning Manager’s decision was that an electronic message sign was not permitted since the applicant’s property line coinciding with the I-25 right-of-way was less than 500 feet.
4. **Planning Commission:** On February 22, 2016, the Planning Commission considered an appeal of the decision by the Current Planning Manager. The Planning Commission over-turned that decision based on a finding that “the term directly abutting could be interpreted to mean perpendicular and parallel to I-25 where there is an adjacent street right-of-way”. Based on that interpretation, the Planning Commission found that the applicant’s property qualified for an electronic message sign. In the course of the discussion, several Planning Commission members asked City staff to amend the code to clarify this issue.

OPTIONS: Attachment A provides options for amending the provisions for electronic message signs in the I-25 corridor. Some of the options directly address the issues raised at the Planning Commission meeting, while others are clarifications to associated signage provisions. The options address the following:

1. Clarifying the definition of “directly abutting” for purposes of determining which properties are eligible for an electronic message sign.
2. Should electronic message signs be allowed only in a “commercial center” and, if so, adding a definition of “commercial center”.
3. Clarifying the meaning of “horizontal presentation” as it relates to signs.
4. Should exceptions to the regulations on electronic message sign within the I-25 corridor be referred directly to the Planning Commission, with final approval reserved for City Council? Currently, such exceptions are heard by the Zoning Hearing Officer. The board of adjustment has the authority to designate one or more hearing officers from within the board to conduct public hearings on matters coming before the board. The designated hearing officer(s) shall have then have the discretion to forward any matter onto the full board of adjustment for an initial public hearing. Within ten days after the conclusion of any hearing conducted by the hearing officer, the hearing officer shall submit proposed findings and order to the board, to the applicant, and to all parties participating in the hearing, which findings and order shall constitute the hearing officer’s final decision.
5. Clarifying that electronic message signs may be internally lit.
6. Does there need to be a limit on number of items of information on an electronic message sign?
7. Should the current provisions for electronic message signs along that portion of Highway 34 included in the U.S. 34 Corridor Plan be updated to be consistent with these revisions for the I-25 corridor and should any other corridor be included in this ordinance revision such as Highway 287?

REVIEWED BY CITY MANAGER:

LIST OF ATTACHMENTS:

- A. City staff memorandum

Memorandum

To: City Council and Planning Commission
From: Brett Limbaugh, Development Services Director
Date: July 26, 2016
Re: Electronic message sign in I-25 corridor

A. INTRODUCTION: This memorandum provides options for amending the Loveland Zoning Code relative to the provisions for electronic message signs in the I-25 corridor. These options are intended to address the issues raised at the February 22, 2016 Planning Commission meeting during the discussion on allowances for electronic message signs on property “directly abutting” the I-25 corridor.

B. OPTIONS:

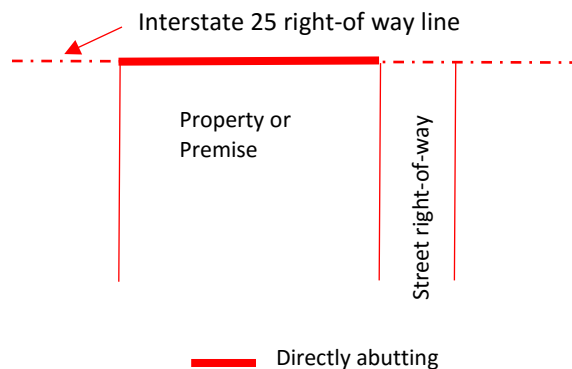
1. Clarifying the definition of “directly abutting” for purposes of determining which properties are eligible for an electronic message sign.

Add the following definition and figure to Section 18.50.020.

18.50.020 Definitions.

“Directly abutting” means a property or premise having a property line coinciding with the right-of-way line of Interstate I-25, as depicted in Figure 18.50.020-1.

Figure 18.50.020-1



Result: Would disallow inclusion of rights-of-way perpendicular to I-25 in determining if a property or premise has 500 feet directly abutting I-25.

2. Should electronic message signs be allowed only in a “commercial center” and, if so, adding a definition of “commercial center”?

Amend paragraph 18.50.120.J as follows.

- J. Electronic Message Signs: Within the I-25 Corridor, electronic message signs shall be permitted only within a planned sign program on premises directly abutting I-25 for more than five hundred lineal feet, provided that the maximum sign area for the electronic message module shall not exceed sixty percent of the total sign face and the sign shall comply with the provisions in Section 18.50.100.A.4. Only one electronic message sign shall be permitted per frontage abutting I-25 within a premise.

Result: Would allow electronic message signs on a premise containing a single business.

Note: If this amendment is not included, then the following definition of “commercial center” should be added to Section 18.40.020.

18.50.020.

“Commercial center” means a premise containing two or more separately owned legal business entities, unaffiliated with each other, including, without limitation, subsidiaries.

3. Clarifying the meaning of “horizontal presentation”. Add the following figure to Section 18.50.020.

“Horizontal Profile” means a sign profile where the width of the sign is a minimum of 50% greater than the height of the sign.

Figure 18.50.020-2



4. Should exceptions to the regulations on electronic message signs within the I-25 corridor be referred directly to the Planning Commission, with final approval reserved for City Council? Currently, variances are heard by the Zoning Hearing Officer, but the code limits the use of variances relative to electronic signs.

Under these proposed procedures, any such exception would not be heard by the Zoning Hearing Officer. The only findings required to approve the exception would be those specific to electronic message signs and not the more general findings for other types of signs. City Council has shown a special interest in electronic message signs along I-25. Currently, granting a variance requires a finding that the regulation would result in an unnecessary and undue hardship. Requiring this finding may not be reasonable or practicable when considering exceptions to the regulations on electronic message signs and may unnecessarily limit City Council discretion regarding electronic message signs along I-25.

Amend paragraph 18.60.040.A. as follows.

18.60.040 Sign variance review criteria.

- A. No variance shall be granted under this paragraph to the provisions in paragraph 18.50.120 for electronic message signs abutting I-25. Any departure from the provisions in paragraph 18.50.120 shall be subject to the requirements set forth for an exception in paragraph 18.50.120.M.
- B. Variances to the requirements of Chapter 18.50 shall not be permitted, except as related to the requirements concerning the setback of a freestanding sign, the spacing between freestanding signs, or the maximum sign area. To approve a zoning variance application to Chapter 18.50, the board must consider the following review criteria and find that each criterion has been met.
 - 1. There are special physical circumstances or physical conditions, including, without limitation, buildings, topography, vegetation, sign structures, or other physical features on adjacent properties or within the adjacent public right-of-way that would substantially restrict the effectiveness of the sign in question, and such special circumstances or conditions are unique to the business to which the applicant desires to draw attention and do not apply generally to all businesses in the area;
 - 2. The variance would be consistent with the purposes set forth in Section 18.50.010 and would not adversely affect the neighborhood or other businesses within the vicinity in which the subject business is located; and
 - 3. The variance is the minimum necessary to permit the applicant to reasonably draw attention to its business.

Amend paragraph 18.50.100.A.4.h.as follows.

18.50.100.A.4

- h. A request for variance to the maximum sign area, height or setback for a sign containing an electronic message module shall be heard by the zoning board of adjustment in accordance with the procedures specified in Chapter 18.60, except that no variance shall be granted under this paragraph to the provisions in paragraph 18.50.120 for electronic message signs abutting I-25. Any departure from the provisions in paragraph 18.50.120 shall be subject to the requirements in

paragraph 18.50.120.M. In addition to the findings specified in Section 18.60.040, before granting any request, the board shall find that:

1. The proposed area, setback and/or height of the electronic message sign module is the minimum required to be fully visible from the adjacent arterial or interstate roadway right-of-way;
2. Traffic safety conditions will not be diminished by the increased square footage, increased height or decreased setback of the electronic message sign module; and
3. There are no reasonable alternatives to the increased size, height, setback and/or design of the electronic message sign.

Add new paragraph 18.50.120.M.

18.50.120.

- M. City Council may, after consideration by the Planning Commission, grant exceptions to the provisions in paragraph 18.50.120 for electronic message signs abutting I-25.
1. Process: Consideration by City Council and Planning Commission shall be in accordance with the provisions for public hearings in Chapter 18.05 and the distance for mailing notice shall be 1,200 feet. No neighborhood meeting shall be required prior to consideration by Planning Commission or City Council.
 2. Findings: In granting any such exceptions, Planning Commission and City Council shall make the findings set forth in paragraph 18.50.100.A.4.h.

The existing findings in paragraph 18.50.100.A.4.h are:

1. The proposed area, setback and/or height of the electronic message sign module is the minimum required to be fully visible from the adjacent arterial or interstate roadway right-of-way;
2. Traffic safety conditions will not be diminished by the increased square footage, increased height or decreased setback of the electronic message sign module; and
3. There are no reasonable alternatives to the increased size, height, setback and/or design of the electronic message sign.

5. Clarifying that electronic message signs may be internally lit.

Amend paragraph 18.50.020E. as follows.

18.50.020 i-25 Corridor

- E. Lighting: Except for electronic message signs approved in accordance with paragraphs 18.50.100.A.4 or 18.50.120.J, signs shall:
1. Be lit by directional, external light sources, internally illuminated letters and logos, or back-lighted raised letters and logos; and

2. The entire sign face shall not be internally illuminated.

6. Does there needs to be a limit on number of items of information on an electronic message sign?

Revise paragraph 18.50.120.G. as follows.

18.50.120 I-25 Corridor.

G. Items of information: all freestanding signs established under this section, except electronic message signs approved in accordance with paragraphs 18.50.100.A.4 or 18.50.120.J, shall be limited to ten items of information. An item of information is a word, an initial, a logo, an abbreviation, a number, a symbol, or a geometric shape.

Result: Limiting items of information on electronic message signs would be impracticable.

7. Should the current provisions for electronic message signs along that portion of Highway 34 included in the U.S. 34 Corridor Plan be update to be consistent with these revisions for the I-25 corridor?

Currently, the provisions for electronic message signs along that portion of Highway 34 included in the U.S. 34 Corridor Plan are generally the same as those in the I-25 corridor. Different regulations on electronic message signs were developed for these two corridors to implement policies contained in corridor plans adopted by City Council, those being the “Northern Colorado I-25 Corridor Plan – Regional Communities” and the “U.S. 34 Corridor Plan”.

AGENDA ITEM: 2
MEETING DATE: 7/26/2016
TO: City Council
FROM: Terry Andrews, Municipal Clerk Services
PRESENTER: Terry Andrews, City Clerk

TITLE:
Global Relay Implementation Discussion

SUMMARY:

City Council requested Staff look at a more transparent Email process regarding their electronic communications. A new software, "Global Relay" has been tested for a few months by City Staff. This new process will allow Council email to be available to the public in "real time" and accessible from outside of the City's computer system. This discussion has two primary functions: 1) To announce in a public meeting the new process; and 2) To answer any questions City Council may have regarding Global Relay.

BACKGROUND:

The current procedure is defined in a policy, established by Resolution #R-101-00 a Resolution adopting an e-mail disclosure policy concerning Electronic Mail Communication of Elected City Officials. On August 2, 2016 City Council will be asked to approve a Resolution that will repeal this resolution. If approved, the new public access will be made available within a couple of days.

Old Process highlights: Council emails are sorted into three categories (with three different retainage times: one year, six months, until administrative purpose is served); they are reviewed by the City Clerk on a daily basis, but must be transferred within 5 days of receipt to a City email account (maintained by the City Clerk); a formal request must be made to view the emails on a city provided computer within the City Clerk's Office (between 8am and 5pm on business days). The current system stores these emails in Proofpoint, which currently has no retention limit.

New Process highlights: All emails will be seen by the public in "real time" (when they are received or sent by Councilors); For Emails that are restricted, i.e. personnel, Attorney Client privilege or other topics identified by CORA will still appear with the sender and recipient, however the subject matter and content of the email will not be viewable. The public can view it from mobile devices as well as a personal computer from their own home, without requesting an appointment with the City Clerk's Office. Since Council will still receive emails through Outlook, the emails will continue to exist in Proofpoint.

Two noteworthy functions of this system. 1) the public have a mechanism to request an email be restricted, however, the email may still be subject to a CORA request (this disclosure will be clearly communicated on the website, where the access information is located); and 2) this process will not affect the "usual" delivery of emails to and from City Council in their Outlook account.

For emails that are inadvertently sent without the restricted language, the City Attorney's office, City clerk, Deputy City clerk, City Attorney's Office or the IT infrastructure manager may place or remove a "restriction status" with just a phone call.

The City Clerk's Office and the City Attorney's office will train City Staff on the use of Global Relay and how to determine the need for restricted access. The City Attorney's Office has prepared an FAQ for the public, which will be available on the City's Website. There will be a public announcement and the local papers will be notified of the new "real time" solution to access City Council Email.

REVIEWED BY CITY MANAGER:

LIST OF ATTACHMENTS:

FAQ – Email Transparency

EMAIL TRANSPARENCY PROJECT - FAQ

DISCLOSURE – Emails to or from City Council are subject to public disclosure under the Colorado Open Records Act (CORA), with limited exceptions. All emails addressed to or sent from City Council, including email addresses, will be visible in an online system in order to promote transparency, except those considered confidential under CORA. Emails with “#private#” in the subject line will appear in the online system, but the content and subject line will be restricted from view. However, the City of Loveland cannot guarantee that an email marked “#private#” will remain private under CORA.

FAQ

What is the Email Transparency Project?

In the interest of increased transparency, the City of Loveland will place all emails sent to or received by City Council after March 8, 2016 online for public review. However, confidential messages pursuant to C.R.S. § 24-72-204 will not be included in the online system.

Why does the City of Loveland make Councilmember email public?

City Council emails, with limited exceptions, are public records under the Colorado Open Records Act (CORA). City Council would like to be as transparent as possible and allow their constituents the ability to review emails to or from City Council about how City business is conducted and how City decisions are reached, so are voluntarily making their email correspondence public.

Will all City Council emails be public via the online system?

With the exception of confidential messages, all emails to or from City Council dating back to March 8, 2016 (Implementation date of project “testing”) will be accessible once the system goes live. Any City Council emails prior to March 8, 2016 may be obtained through a written public records request submitted to the City Clerk’s Office ([access the City's Public Records Request form by clicking here](#)). Emails restricted in the online system are generally limited to those that must be kept confidential by law. Examples include communications that are attorney-client privileged, personnel-related, or include proprietary information. Citizens who wish to communicate with a councilmember confidentially about matters that are not public business may elect to have their email kept private. The City will attempt to keep such emails private, however, such communications may or may not be subject to public inspection under the Colorado Open Records Act.

How do citizens request an email to a City Councilmember be kept confidential and not available to the public via the online system?

The system will automatically mark a message “restricted” any time the term “#private#” appears in the subject line of an email. The hash-tag must be used and “#private#” must be spelled correctly for the system to recognize the request for a restricted message. Citizens should note that any email addresses will still be visible, but the content and subject line of their message will be restricted. As stated above, the City will attempt to keep such emails private, however, such communications may or may not be subject to public inspection under the Colorado Open Records Act.

If I CC or BCC people on an email to Councilmembers, will those recipients appear in the system?

Yes. All email addresses on an email to or from Council will be visible in the system, even if they are in the BCC field, as the blind copy function will not work. Note that even email addresses on emails with #private# in the subject line will be visible.

Is data ever deleted from the system?

Emails cannot be deleted manually from the system by City Councilmembers or City employees. However, the City may restrict an email in the system if it is determined to be confidential under C.R.S. § 24-72-204. All City Council emails will be retained per City's current email retention policy.

How can I access the system?

Go to: public@cityofloveland.org

Login using the following:

 Password: (this information will be released after the go live date)

Click "search" to view all emails within the date range at the top of the page, or use the search tools to locate specific communications.

CITY COUNCIL E-MAIL TRANSPARENCY PROJECT

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GLOBAL RELAY

TUESDAY, JULY 26, 2016



GOALS

TRANSPARENCY IN CITY COUNCIL EMAIL

AUTOMATED PROCESS

GREATER ACCOUNTABILITY TO PUBLIC

RETENTION ADHERENCE



COMMUNICATIONS EFFECTED

CITY COUNCIL MEMBER TO CITY COUNCIL MEMBER
STAFF TO CITY COUNCIL & CITY COUNCIL TO STAFF
PUBLIC TO CITY COUNCIL & CITY COUNCIL TO PUBLIC



TIMELINE

implementation Date:	March 8, 2016
Test Period:	4 Months
Go Live for public view:	August 3, 2016
Minimal Impact:	Change in process - insertion of #private# in Subject line
Training:	City Council, July 26, 2016 Staff, July 27, 2016 – August 3, 2016

CCMAIL PROCESS TRANSITION

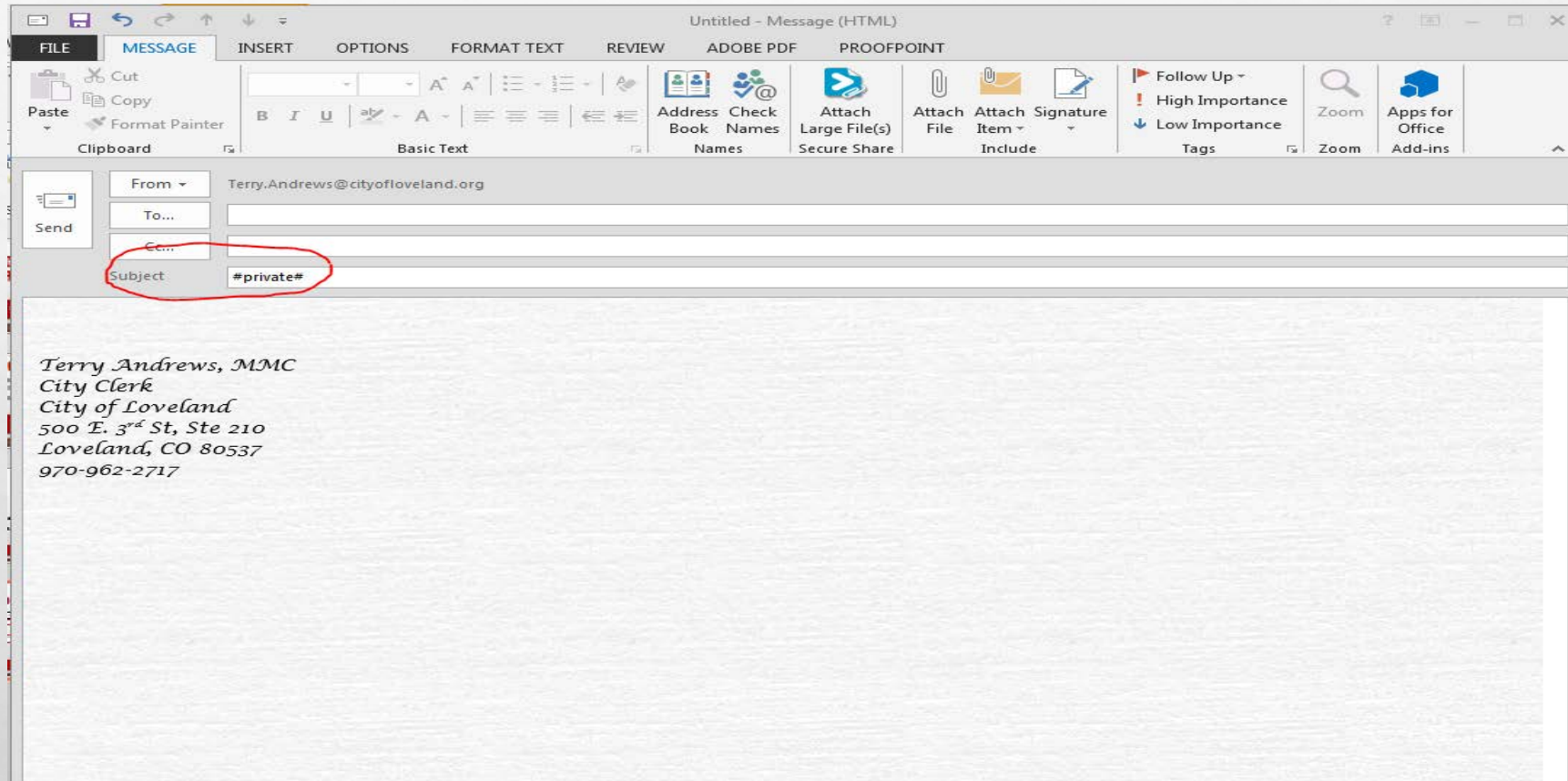
CURRENT PROCESS FOR CC MAIL



PROPOSED PROCESS FOR CC MAIL



#PRIVATE#



CAUTIONS

- EMAILS ARE AUTOMATICALLY SORTED BASED ON #PRIVATE# IN THE SUBJECT LINE
- PROCESS FOR EMAILING CONFIDENTIAL ITEMS TO CITY COUNCIL
 - ORIGINAL EMAILS – INSERT #PRIVATE# INTO SUBJECT LINE
 - REPLY & REPLY ALL – ENSURE #PRIVATE# IS STILL CONTAINED IN THE SUBJECT LINE
 - FORWARD – CAUTION, CAUTION, CAUTION #PRIVATE# IS ONLY PROTECTED ON THE CITY SITE

QUESTIONS?



AGENDA ITEM: USED BY AGENDA MANAGEMENT
MEETING DATE: 7/26/2016
TO: City Council
FROM: Leah Browder, Public Works
PRESENTER: Leah Browder, Public Works Director
 Jeff Bailey, PE, PTOE, City Engineer

TITLE:
 TRANSPORTATION AND STREET OVERVIEW

RECOMMENDED CITY COUNCIL ACTION:
 THIS IS AN INFORMATIONAL ITEM

OPTIONS:

1. After consideration and discussion of the information provide guidance for 2017 capital and/or operating budget development.
 2. Refer back to staff for development of specific areas of interest.
 3. Hold general discussion with no specific outcome.
-

SUMMARY:

This is an informational presentation intended to help the City Council prepare for development of the 2017 budget.

BUDGET IMPACT:

- Positive
 Negative
 Neutral or negligible

This item provides information to support City Council decision making regarding capital project planning.

BACKGROUND:

As various transportation issues have arisen during 2016, City Council requested a Transportation and Street Overview to prepare for discussion and development of the 2017 Budget. Tonight's information is intended to respond to this request by presenting an overview of capital projects organized by east-west connectivity and north-south routes; bridge assessment and needs; multi-modal considerations; and pavement condition status. Also included is an update on traffic congestion and safety data collection.

REVIEWED BY CITY MANAGER:

LIST OF ATTACHMENTS:

1. Staff Memo
2. PowerPoint Presentation

Memo

Public Works Department

To: Honorable Mayor and City Council

Via: Stephen C. Adams, City Manager

From: Leah Browder, Public Works Director
Jeff Bailey, PE, PTOE, City Engineer

Date: July 19, 2016

Re: July 26, 2016 City Council Meeting
Capital Transportation Projects

Summary:

This memorandum is intended to support the staff presentation scheduled for the July 26, 2016 City Council Study Session. The Transportation and Street Overview Study Session item is a focus topic presentation to support City Council decision making for the 2017 Budget.

While the focus on Loveland's transportation system is often capital project construction for increased roadway capacity, this year's overview broadens the dialogue to include maintenance and day-to-day operations as all these aspects must be attended to ensure optimal operation of the street network.

For Loveland to meet the system challenges anticipated with projected growth and demographics, full implementation of a multi-modal system maximizing bicycle, pedestrian, transit and rail (if available) dimensions will be necessary. Therefore, these topics are also included.

Background and Current Approach:

Loveland is projected to experience substantial ongoing growth and change that will continue to burden the City's transportation system. The City's 2035 Transportation Plan provides a formal framework for project planning for a 20-year horizon—it serves as the initial starting point for every year's capital project planning effort. The Transportation Plan is based on extensive technical analysis and public input and looks carefully at the specifics of Loveland's projected growth during the planning window, including projections for all adjoining entities through a specially developed travel model incorporating projected traffic volumes on individual streets. This model is developed through intense coordination between Public Works, Community and Strategic Planning and the City's regional partners through the North Front Range Metropolitan Planning Organization.

The Transportation Plan is formally reviewed and revised approximately every five years to include the latest available information, technical analysis, growth patterns and public input in order to provide a document that guides transportation decision-making toward a future that is desirable to the community.

The 2035 Transportation Plan was adopted in 2012 and the next formal Transportation Plan update will begin formally in 2017 to develop the City's 2040 Transportation Plan.

In the meantime, due to rapidly changing conditions, the transportation system is continually reviewed and evaluated considering changing traffic patterns, transportation system demands and public input. As noted by CDOT Regional Director, Johnny Olson, US Highway 34 is changing more quickly than any agency had projected and in ways not predicted. This has caused the re-prioritization of projects in the 2035 Transportation Plan such as the acceleration of the 37th Street connector to 2019 from the originally planned 2026 construction date.

In this regard, beginning in July, City staff will begin collecting data about traffic congestion using Open City Hall on the City website. The first area of focus will be traffic congestion with questions seeking information about top locations of concern for our citizens. Respondents will also be asked about alternate routes and what issues are now arising on those alternate routes as a result of increased, diverted traffic. The information collected will help further formulate short-term interventions as well as providing the first round of data for the 2040 Transportation Plan update.

Key outcomes of the Transportation Plan update include identification of projects to address traffic forecasts AND programming of those projects in consideration of public input as well as given anticipated funding types and amounts. These projects primarily include roadway widening, new roadway segments, additional turn lanes, and intersection control (traffic signals, roundabouts, etc.) enhancements.

On the issue of transportation project funding, Loveland is no different than any other growing city in that there are many more capital needs than there is funding available. The Transportation Plan is structured for maximum flexibility--providing a menu of priority projects, appropriating annual contributions toward an ultimate estimated project cost, ready to move forward quickly or undergo revision should unexpected funding opportunities arise, growth projections and/or traffic impacts differ from reality, projected funding types change or fail to materialize, and/or unanticipated issues arise.

It is also important to note that carefully considered selection criteria are also applied to support objective, transparent project prioritization recommendations. This approach helps to ensure fairness across neighborhoods and support our citizens in understanding why one project might be selected over another. Because priorities change and each project scoring exercise represents a snapshot in time, the prioritization list is reviewed and revised as necessary to keep the priority list as current as possible.

Currently applied criteria and weighting is as follows:

RATING AREA	PERCENT OF SCORE
System Continuity/Congestion Mitigation	20%
Safety Enhancement	25%
Multi-modal Enhancement	5%
Environmental Impact	10%
Implementability	15%
Economic Impact	20%
Regional Significance	5%
TOTAL	100%

The projects included in the Transportation Plan are categorized as Major and Minor projects. All projects are also broken down into logical phases so that they can be better balanced with projections for type and amount of anticipated future funding. Additionally, this approach provides for phased construction of large projects so that roadway users can experience gradual improvement over time rather than waiting longer periods of time for ultimate improvements to be constructed.

Transportation Street Network Description:

The roadway network is designed to connect local streets to collector streets to arterial streets. Each roadway classification has its own purpose in the system, primarily related to the amount of traffic it carries. Roadway classifications are determined based on long-term traffic projections. The classifications also determine the number and spacing of access points. Some roads change classification depending where you are at on them. For example, Madison Avenue is a Major Arterial from US 34 to 29th Street, but is a Minor Arterial north and south of that section.

In Loveland, the arterials are on the one-mile grid, the collectors are on a half-mile grid and connect to the arterials, and the local streets (whether residential, commercial or industrial) serve individual properties and connect into the collector system. The classification of the street also determines the amount of direct access allowed, with arterials being the most restrictive to support efficient traffic flow.

The classification of the street determines who is responsible for its construction and funding. Based on adopted policies, development is responsible for constructing local and collector streets, including those within and adjacent to their developments. In addition, as part of the Adequate Community Facilities (ACF) requirements, all development must be connected to the nearest arterial by a 34-foot-wide paved roadway. As part of the review and approval process, each proposed development is required to analyze and propose the roadway network necessary to serve their development. It is generally required that the street system be designed to ensure that a development's projected traffic has less than a 10% impact on existing conditions. Due to these requirements, only arterial streets are included in the funding section of the 2035 Transportation Plan.

Geographic Challenges: Loveland has geographic challenges that create unique circumstances when considering east-west connectivity. Reservoirs and lakes prevent the extension of roadways. For example, Boyd Lake runs from 71st Street to US 34, a distance of four miles with no ability to continue major roadways across (i.e. 57th Street, 43rd Street, 37th Street and 29th Street). Also a challenge to east-west connections are three existing Railroad Companies (BNSF, Union Pacific, and Great Western). A specific example of this is the gap from 37th to 57th Streets and from 1st Street to SW 14th Street.

Loveland's geographic constraints are addressed primarily through planning and access management along the existing key corridors, as well as strategically timed and coordinated widening improvements and connections. These strategic coordinations must be reviewed carefully when new project ideas or schedule changes are raised as revisions to the adopted Transportation Plan can cause the need to modify other, related improvements.

While it may be possible to construct bridges or causeways over geographic barriers, significant cost implications make these possibilities impractical. For instance, the cost for a four-lane arterial street is approximately \$2,500 per linear foot while four-lane bridge construction can cost \$41,600 per linear foot.

In order to generate improvements to east-west connectivity, the 2035 Transportation Plan and 10-Year Transportation Capital Improvement Program (CIP) include projects along key corridors and routes that

result in a transportation system that creates and enhances east-west options and alternatives. Considerations include the significant number of destinations on the east side of the city compared to the west side of the city where more trips tend to originate. Key examples of this are 29th Street to Madison Avenue to US Highway 34 and State Highway 402 (14th Street). In both of these examples, the 2035 Transportation Plan includes numerous projects that address the overall need for improvements, yet focus on location specific solutions. As an example, State Highway 402 needs to be widened from US 287 to I-25, but does not need to be widened to four lanes along its entire length. In the 10-Year CIP, the projects are focused on addressing specific locations within the larger project area where more congestion is occurring, increasing capacity until the larger improvements can be implemented. For instance, the extension of the widening of US 34 from Denver Avenue east to Boyd Lake Avenue.

Transportation Capital Program Funding Overview:

Funding sources presented in the 2035 Transportation Plan include only: Capital Expansion Fees for Streets (Street CEF's); Collector Street Equivalents; Other (Primarily Federal and State Grants); Centerra Metro District Funds (for improvements identified in the Master Finance Agreement); and General Fund. Project funding and construction plans may be modified if additional grants are secured, and/or additional funding is recommended by the City Manager's Office and approved by the City Council (e.g. TABOR Reserve, Special Project Reserve, etc.).

To maintain an accurate financial plan, the Transportation Plan not only presents funding formulas for City-constructed projects, but also reimbursement schedules for projects constructed as part of specific developments eligible for reimbursement under the City's policies. The City's projects include planning studies, construction of roadway improvements, as well as traffic signal additions and interconnections, bike facilities, sidewalk additions, and other on-going efforts identified in the City's 2035 Transportation Plan. Also included is annual funding set aside in case matching opportunities become available through grants or other outside sources.

Impact of Funding Challenges: The 2035 Transportation Plan includes a list of projects necessary to be completed by 2035, based on analysis of growth projections, in order to meet transportation goals. Funding is limited and contingent on sources. Many projects are contingent on growth. Therefore, the improvements tend to lag behind the need as all of the funding is collected before the project is constructed.

Project funding sources assumed in the 2035 Transportation Plan are as follows:

	<u>2012</u>	<u>2016</u> <u>(2012 Dollars Updated to 2016)</u>
City General Fund	\$ 34,099,009	\$ 37,695,175
Capital Expansion Fees (Streets)	\$129,886,011	\$143,584,112
Collector Street Equivalent	\$ 44,009,280	\$ 48,650,610
CDOT Funds	\$ 37,784,700	\$ 41,769,570
Centerra Metro District Funding	<u>\$218,644,630</u>	<u>\$241,703,436</u>
TOTAL	\$464,644,630	\$513,402,903

CAPITAL PROJECT OVERVIEW

East-West Connectivity Projects:

With US Highway 34 being the major east-west transportation route for Loveland, a focus on improvements to support this roadway's ability to carry projected traffic is a priority. However, it is not possible, either from a constructability or cost perspective, to expand this central corridor so that it is large enough to eliminate traffic congestion. Therefore, it will be necessary for the City to consider how best to integrate all modes of travel to effectively manage traffic demands. Also important, is the concurrent development of alternate east-west routes to provide viable, efficient options.

The 10-year Capital Plan includes 20 projects that will help improve east-west connectivity in Loveland. Of those 20 projects:

- 12 are fully or partially funded at an estimated total of over \$15,000,000
- 2 of the 12 projects (34 eastbound widening from Denver to Boyd Lake and 37th and Monroe Roundabout) are still in need of an estimated \$6,400,000
- 2 of the 12 projects are developer funded (Mountain Lion Drive Connection) and CDOT funded (402 and County Road 9E Intersection Improvements) with project costs unknown and not included in the \$14 million total above
- 1 developer project(Boyd Lake and 15th Street) is not currently planned and thus unfunded
- 6 of the 20 priority City projects are not currently funded
- Cost estimates for those 6 unfunded projects total over \$31,000,000 in 2016 dollars
- Costs have not yet been determined for 2 of the projects

The following summary charts provide an overview of projects supporting the City's east-west transportation routes:

US Highway 34		
Project	Year	Estimated Cost <i>Funded/Unfunded</i>
34 Westbound Widening (Denver to Boyd Lake)	2018	\$ 5,000,000
34 and Boyd Lake Intersection Improvements Boyd Lake and Mountain Lion Drive Roundabout	2017	\$ 3,200,000
Mountain Lion Drive Connection (Developer Project)	2016	Developer Project
Boyd Lake and 15 th Street Roundabout/Bridge Replacement	YTBD	Developer Project
34 Eastbound Widening (Denver to Boyd Lake)	YTBD	\$100,000/\$ 5,900,000
34 Widening (Boyd Lake to I25)	YTBD	\$ 15,000,000
34 Improvements (I25 to Centerra Parkway)	YTBD	\$ 3,400,000
34 and Taft Avenue Intersection Improvements	2019	\$ 1,800,000
34 and 287 Intersection Improvements	YTBD	YTBD
TOTAL		\$10,100,000+/\$24,300,000+

At the July 5, 2016 City Council Meeting, additional information was requested about status and timing of improvements north of US34 on Boyd Lake Avenue. Future plans include bridge widening (which require the bridge be replaced), an underground pedestrian crossing and a roundabout dependent on development in the area. At a meeting on July 8, 2016, McWhinney representatives indicated a willingness to discuss this improvement project. However, with recent acceleration of other priority roadway projects (Kendall Parkway, Mountain Lion Drive and Sculptor Drive), the company's ability

to advance another \$4 to \$5 million project before necessitated by development in the area is likely limited.

Highway 402 (14th Street SW)		
Project	Year	Estimated Cost Funded/ <i>Unfunded</i>
14 th Street SW Resurfacing (Taft Avenue to BNSF Railroad)	2016	\$ 570,000
402 and County Road 9E (Sculptor Drive) Intersection Improvements	YTBD	CDOT Project
TOTAL		\$ 570,000+

29th Street		
Project	Year	Estimated Cost Funded/ <i>Unfunded</i>
29 th and Beech Drive New Traffic Signal	2017	\$ 500,000
29 th and Madison Intersection Improvements	2020	\$ 500,000
TOTAL		\$ 1,000,000

37th Street		
Project	Year	Estimated Cost Funded/ <i>Unfunded</i>
37 th Connection from 287 to Lincoln Avenue*	2019	\$ 2,500,000
37 th and Monroe Roundabout	2021	\$ 500,000/\$ 500,000
37 th and Boise Intersection Roundabout	2026	\$ 2,300,000
TOTAL		\$3,000,000/\$ 2,800,000

* Requires companion stormwater project for Dry Creek estimated cost \$1.7m to be funded through the Stormwater Enterprise Fund.

43rd Street		
Project	Year	Estimated Cost Funded/ <i>Unfunded</i>
43 rd Extension from Smith Park Court to Duffield Avenue (with culvert)	YTBD	<i>YTBD</i>
TOTAL		<i>YTBD</i>

57th Street		
Project	Year	Estimated Cost Funded/ <i>Unfunded</i>
57 th and Taft Intersection Improvements	2020	\$ 500,000
57 th Street Reconstruction (Wilson Avenue to Taft Avenue)	YTBD	\$ 1,500,000
57 th Improvements (Taft Avenue to 287)	YTBD	\$ 8,900,000
TOTAL		\$ 500,000/\$ 10,400,000

North-South Connectivity Projects:

Loveland's significant north-south connecting routes must also keep pace to provide the necessary interplay with the east-west road network. The 10-year Capital Plan currently includes 6 projects for north-south connectivity. Of those 6 projects:

- 3 are anticipated to be fully funded totaling an estimated \$2,400,000, including one developer project (cost not included in this estimated total)
- 2 are partially funded at \$2,600,000, but require an additional \$2,800,000
- 1 of the six projects is not currently funded at an estimated cost of \$8,000,000 in 2016 dollars

The following summary charts provide an overview of north-south connector route projects:

Taft Avenue		
Project	Year	Estimated Cost Funded/ <i>Unfunded</i>
Taft Avenue Bridge and Road Widening (8 th to 11 th)	2016	\$ 1,600,000
Taft Avenue Road Widening (11 th to West Shore Drive)	YTBD	\$ 8,000,000
Taft Avenue Road Widening (South of 22 nd Street SW)	YTBD	Developer Project
TOTAL		\$1,600,000+/\$ 8,000,000

Garfield Avenue		
Project	Year	Estimated Cost Funded/ <i>Unfunded</i>
Garfield Roadway Improvements	2017	\$ 800,000
TOTAL		\$ 800,000

Madison Avenue		
Project	Year	Estimated Cost Funded/ <i>Unfunded</i>
Madison Avenue Improvements (29 th Street to 37 th Street)	2021	\$ 500,000/\$ 1,100,000
Madison Avenue Improvements (Silverleaf to 29 th Street)	2023	\$ 2,100,000/\$ 1,700,000
TOTAL		\$ 2,600,000/\$ 2,800,000

System Gap Projects:

Gap projects are not necessarily directly related to major east-west or north-south roadways, but may provide relief on those major system roadways by serving as alternate connections.

System Gap Projects		
Project	Year	Estimated Cost Funded/ <i>Unfunded</i>
Duffield extension from Coral Burst Drive to 57 th Street	YTBD	Developer Project
11 th Street extension from Redwood to Gorom	YTBD	Developer Project
Sculptor Drive	2018	Developer Project
Byrd Drive	2016	Developer Project
Kendall Parkway	2018	Developer Project
Boyd Lake extension from 5 th Street to Hwy 402	YTBD	YTBD

MULTI-MODAL CONSIDERATIONS

As noted above, it is not possible, either from a constructability or cost perspective, to expand Loveland roads to be wide enough to accommodate all projected traffic to the point of eliminating congestion. Therefore, it will be necessary for the City to consider how to support all modes of travel to effectively manage traffic demands as best as possible.

Bicycles and Pedestrians:

After extensive modeling, collaboration, outreach, and analysis, the City's 2012 Bicycle and Pedestrian Plan identified a top 300 priority projects from the thousands possible to more fully develop this aspect of the transportation network.

2012 cost estimates range from a low of \$6.7 million to a high of \$13.6 million in City funding required to achieve the priority projects. It is important to note that the sidewalk estimates are based on standing policy direction to pursue pathways on only one side of the road. It is also important to state that many of the projects will require cooperation and collaboration with others such as the County, developers, etc. to realize the projects. External funding ranging from \$6.1 million to \$12.3 million is required in addition to City funding.

Therefore, total project funding ranges from an estimated \$12.8million to \$25.9 million to achieve the Plan's top 300 priority projects.

Staff looks forward to hearing from CanDo Loveland as it is anticipated that this group's hard work will provide updated information about public input they have gathered about priorities and needs.

The chart below summarizes planning level cost estimates for the Bicycle and Pedestrian Plan in 2012 dollars.

	Bicycle Plan (\$ million)		Pedestrian Plan (\$ million)		Total (\$ million)	
	Low	High	Low	High	Low	High
High Priority	\$1.0	\$2.5	\$0.9	\$1.5	\$1.9	\$4.0
Medium Priority	\$1.1	\$2.6	\$0.9	\$1.5	\$2.0	\$4.1
Low Priority	\$0.9	\$1.7	\$0.9	\$1.8	\$1.8	\$3.5
Enhanced Crossings					\$1.0	\$2.0
SubTotals	\$3.0	\$6.8	\$2.7	\$4.8	\$6.7	\$13.6
Others Responsibility	\$3.0	\$6.1	\$3.1	\$6.2	\$6.1	\$12.3
Totals	\$6.0	\$12.9	\$5.8	\$11.0	\$12.8	\$25.9

Beginning in 2018, the annual allocation is scheduled to drop from \$100,000 annually to \$50,000. This will likely result in construction projects occurring every second or third year to increase the cost-effectiveness of projects and better bid results. If successful, a 2017 departmental budget request would result in an increased annual allocation of \$260,000 and projects would then be possible every year.

Transit:

In order to maximize the City of Loveland's Transit System, a significant update to the 2009 Transit Master Plan is necessary. Preliminary interest in a collaborative Plan update effort with Fort Collins, Greeley and the North Front Range Metropolitan Planning Organization has been indicated. Staff from

these organizations coordinate regularly seeking to support regional integration for transit users. While there is interest in a more regional approach at the staff level, formally undertaking regionalization would be a complex and long-term initiative. With Loveland's system being the most underdeveloped of the three systems, a significant investment would be required to strengthen the operation to the point where it could be viably integrated into a regional operating system.

In the meantime, however, a variety of steps are planned both for this potential and to support enhanced multi-modality for the larger transportation system:

1. National recruitment for a new manager;
2. Request for proposals for a contractor to potentially:
 - a. Manage operations;
 - b. Complete a Master Plan update (presumably strengthened by experiencing day-to-day management activities);
 - c. Provide recommendations for a system where the main goal is to take cars off the road including analysis of a direct route rather than a loop route approach;
 - d. Further investigate potential partnerships with the Thompson School District for student ridership and with tourism aspects (e.g. hotels and popular destinations) to expand economic development possibilities.
3. Energy efficient fleet analysis to determine whether an investment in Clean Natural Gas is worthwhile to continue to secure federal funding for bus purchases.

Commuter Rail:

While no immediate possibilities seem available for this alternate transportation mode at this time, staff continues to track this topic as ideas arise and is prepared to participate further should viable options come into play.

TRANSPORTATION SYSTEM MAINTENANCE NEEDS

The focus on the City's transportation system is often on capital construction projects to expand roadway capacity. However, taking care of our existing systems is a necessary obligation. For all the ground that may be gained by building capacity improvements, much more can be lost if our bridges are closed to traffic or our roads fall into such disrepair that they are unpassable or deteriorated to the point that travel becomes inefficient or that vehicle damage occurs.

Bridge Replacement and Maintenance Needs:

The City of Loveland has approximately 87 bridges in its inventory with 39 subject to every-two-year Federal inspections and rating criteria performed locally by CDOT. CDOT inspectors evaluate each bridge and assign a Sufficiency Rating determined by the structure's adequacy and safety, serviceability, and functional obsolescence.

As a result of CDOT inspection and rating, four Loveland bridges have ratings less than 50 requiring replacement within the next 10 years or sooner. The chart below provides an overview.

Location	Estimated Project Cost	Year
8 th Street over Big Barnes	\$1,500,000	2016
1 st and Garfield	\$1,000,000	2017
Taft Avenue Over Big Thompson	\$3,000,000	2020
Boyd Lake Avenue	\$1,500,000	2026

Of the 39 bridges subject to Federal requirements, nine are more than 50-years-old and 19 are more than 25-years-old. In order to extend the life-expectancy of these significant infrastructure assets a regular, systematic maintenance program is requested in the Public Works 2017 budget submission. If funded at the requested level of \$260,000 annually, this program would allow for power washing, railing painting, and structural coating (paint or stain) on the parapets. The program would also allow for periodic rehabilitation including guardrail upgrades, waterproofing, and structural concrete repairs, in order to postpone the need for full, cost intensive replacement.

As Public Works undertakes implementation of formal asset management practices, bridges represent an initial area of focus. In conjunction with the aspects described above, a full inventory of all 87 structures is underway. This effort will result in a GIS map layer including characteristics of each structure, maintenance and repair history, and maintenance and repair prioritization and schedule.

Pavement Maintenance Needs:

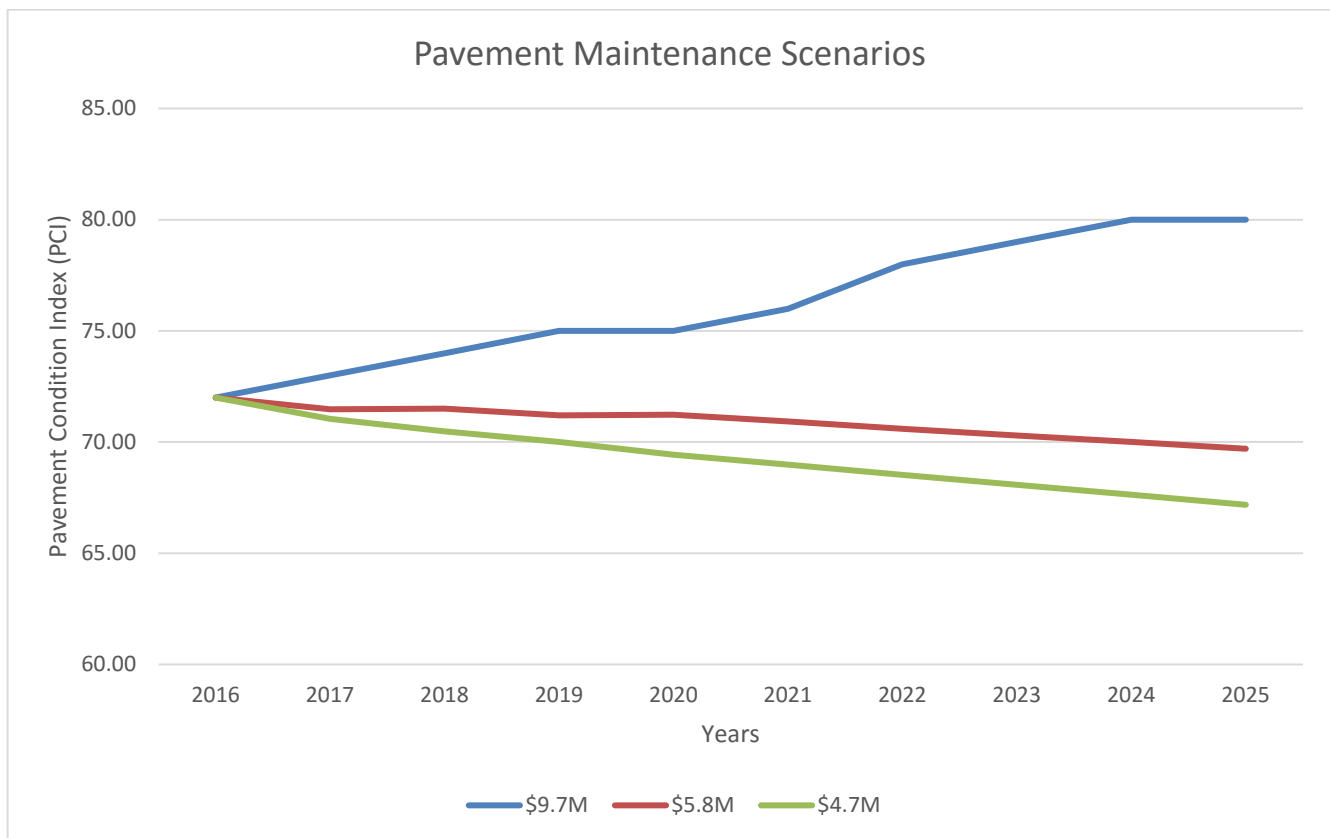
The City street system consists of 7,100,100 square yard of pavement surface and 340 centerline miles of streets valued at \$772,000,000.

The City of Loveland prides itself on the pavement condition of its streets. In order to rate roadway services, pavement condition is observed and scored for every street in the city. Loveland's entire street system was inventoried by a consultant in May of 2014. The results of that inventory rated the overall Pavement Condition Index (PCI) at a 72 on a scale of 100. Loveland's PCI in 2011 was 78.

While the City allocates approximately \$4.7 million annually to pavement maintenance, the street network has grown by an average of 180,500 square yards per year since 2004. Some construction costs for this same period have increased by as much as 29% (Engineering News Record) meaning that our pavement maintenance efforts continue to lose ground.

Three pavement maintenance scenarios are show in the chart below:

1. Increasing the current annual allocation by \$5.0 million to \$9.7 million will improve the PCI rating from a 72 to an 80.
2. A \$1.1 million annual increase is necessary to maintain the current PCI of 72 for the next 10-years.
3. Maintaining a \$4.7 million annual allocation will result in a deterioration of pavement condition to an estimated 67 PCI at the end of the 10-years.



TRAFFIC CONGESTION AND TRAFFIC SAFETY

In preparation for undertaking development of the 2040 Transportation Plan, and to help prioritize immediate needs, two survey initiatives are planned for the City website using Open City Hall.

First will be a focus on Traffic Congestion. As noted by CDOT in relation to US34, Loveland's roadways are changing more quickly and unpredictably than anyone had imagined. While we know the 34 is a top area of concern for our citizens, we would like to know more about the details of the locations and congestion challenges. We would also like to know what alternate routes travelers are using and what new issues may be arising as a result. Observations might include items like, "a protected left hand turn is needed at 1st and Denver now because traffic is so heavy that there is no break where a car can turn left without a green arrow." This data will be used to prioritize top problem areas and will serve as the basis of a peer and consultant review to include a signal timing analysis and potential future capital improvement projects.

Secondly, we are partnering with the Loveland Police Department to seek information about traffic safety concerns. As both our population and Northern Colorado tourism grows, many more drivers are on our roads. What are our citizens experiencing that they would like us to focus on? These topics might include speeding, red light running, aggressive driving, distracted driving, slow drivers, etc. We also hope to learn about locations where these behaviors are concerning most often. This survey data will be used in conjunction with a major intersection safety analysis that is underway. After prioritization, Public Works and the Police Department will work together to develop customized plans for priority locations that apply engineering, enforcement and education (the 3 E's) in an integrated strategy to tackle the challenges identified by survey respondents.

Conclusion:

Loveland's projected growth and demographic changes will require the design and implementation of a multi-modal transportation system that provides options for all modes of travel: vehicles; bicycles; and pedestrians, as well as transit options for our growing millennial and baby boomer populations.

While the tendency is to focus on capital construction projects that will increase roadway capacity, it is also necessary to invest in the maintenance and rehabilitation of the already significant system in place. To construct new without maintaining the old could result in unsafe conditions for the traveling public and exorbitant catch-up expenses and replacement construction, perhaps impossible to surmount if allowed to accumulate.

The City's Transportation Plan provides the baseline and guidance for 20 years of transportation system capital projects and their funding. It is an intricate inter-weave of technical analysis; public input; and the proper application of restricted funding sources, balanced with timing and related required projects. While structured to provide as much flexibility as possible, the Transportation Plan seeks to ensure annual appropriations of appropriate monies into each priority project so that all make progress toward phased project construction and completion.

Please note, modifying project priorities or schedules requires review of the full program to ensure that existing related projects are revised as necessary and to identify other related issues that may need to be addressed as a result of changes.

We look forward to City Council's current thoughts in these areas and the opportunity to update our approach to reflect any changes in policy directed by City Council.

If you have any questions or comments, please do not hesitate to contact Jeff Bailey by phone at (970) 962-2551 or email at jeff.bailey@cityofloveland.org or Leah Browder at 970-962-2520 or at leah.browder@cityofloveland.org. Thank you.

Transportation and Street Overview

City Council Study Session

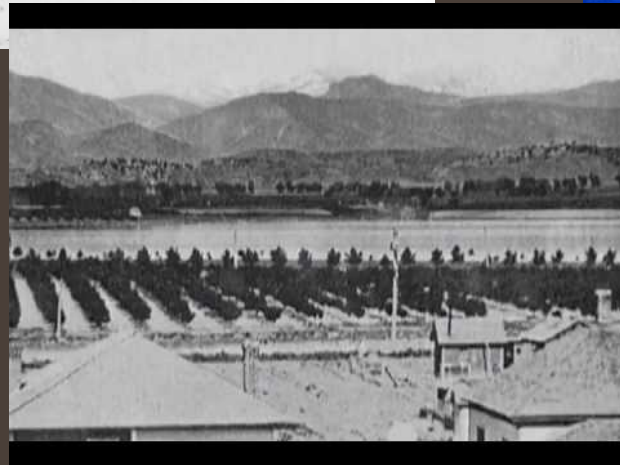
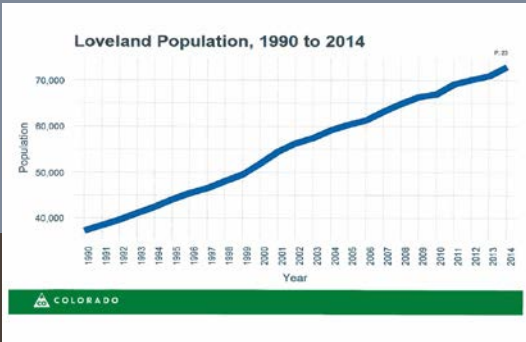
City of Loveland

Public Works Department

July 26, 2016

From Little to Big

1882	250
1950	6,700
2016	74,000
2045	133,000



U.S. DOT: Beyond Traffic 2045 Trends and Choices

Population Increase

2015: **320 million people**
2045: **390 million people**

In 30 years our population is expected to grow by about

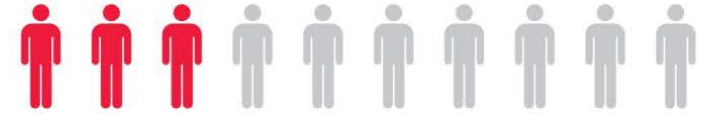
70 million

... that's more than the current populations of



Older Americans — Redefining Longevity

By 2045, the number of Americans over age 65 will increase by



77%

About **one-third of people over 65** have a disability that limits mobility. Their access to critical services will be more important than ever.

Millennials — Shaped by Technology

There are **73 million Millennials** aged 18 to 34. They are the first to have access to the internet during their formative years and will be an important engine of our future economy.

Millennials are driving less. By the end of the 2000s, they drove over **20% fewer miles** than at the start of the decade.



Income Inequality

10% of the population takes home **one-third** of our national income.

Transportation is the **second-largest** expense for U.S. households.



Bumper-to-Bumper

On average, we spend

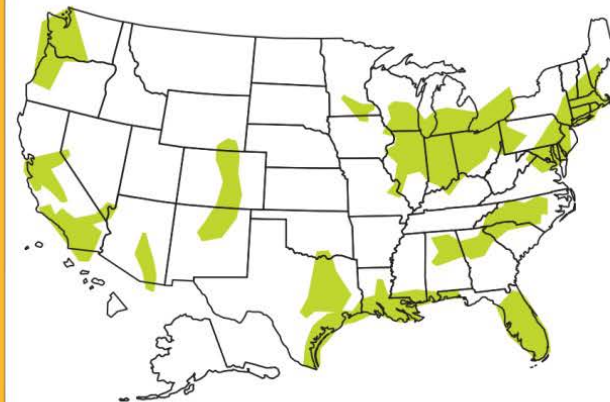
over 40 hours



stuck in traffic each year

The annual financial cost of congestion is

\$121 billion



Megaregions and Shifts in Population Centers

11 megaregions are linked by transportation, economics, and other factors.

They represent over **75%** of our population and employment.

In 2014, **365,000** people moved to the South—up **25%** from 2013—and moves to the West doubled.

Tonight's Roadmap

- 2035 Transportation Plan
- Capital Projects
 - East-West Connectivity
 - North-South Network
 - System Gaps
 - Bridges
- Multi-Modal Mobility
 - Bicycles and Pedestrians
 - Transit
- Roadway Maintenance
- Traffic Congestion
- Traffic Safety

City of Loveland 2035 Transportation Plan

Fiscally Constrained

125+ Total Projects (\$464m):

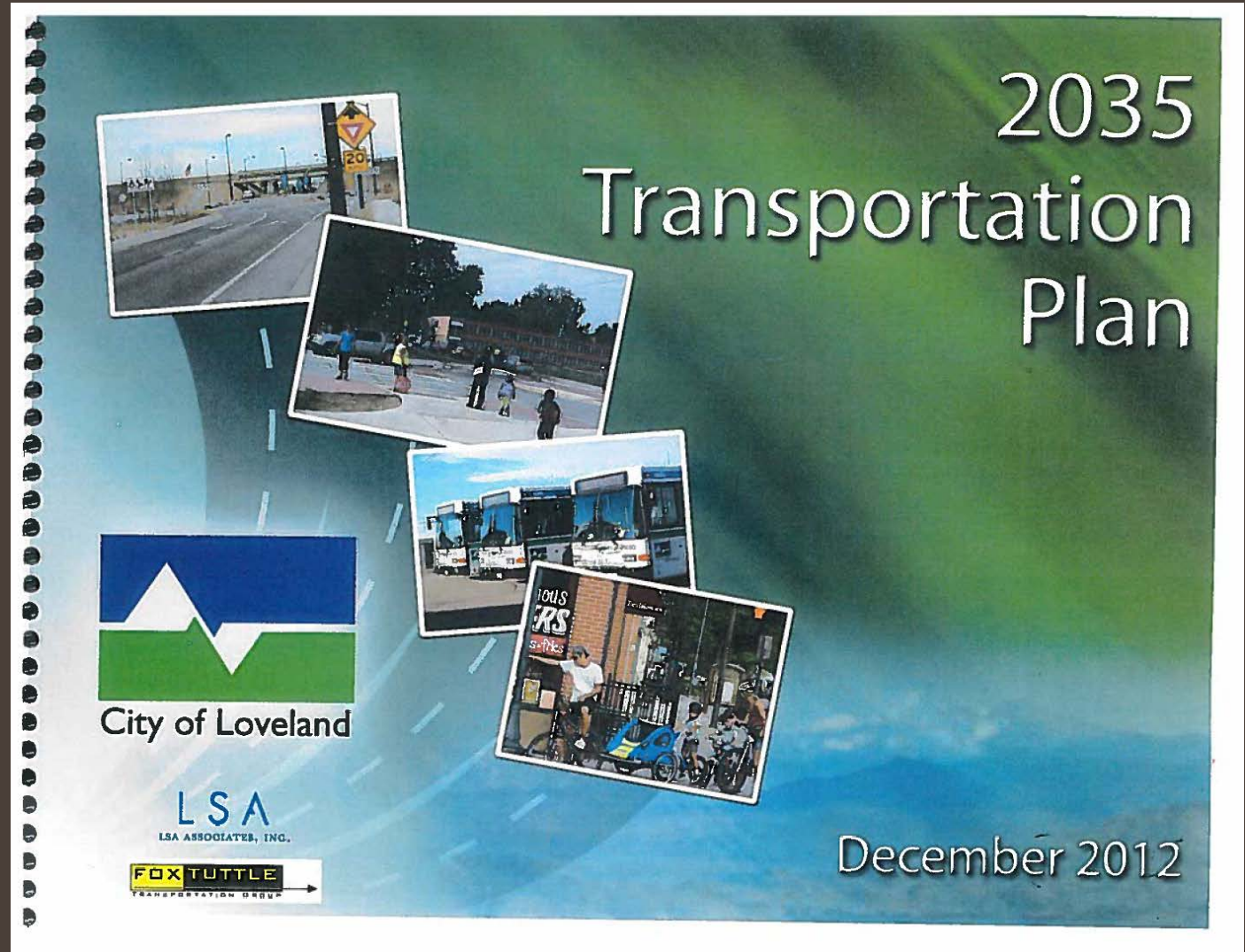
24 City Street Projects (\$107m)

13 State Highway Projects
(\$64m)

41 Intersection Projects (\$50m)

5 Other Priority Areas (\$25m)

43 Centerra Projects (\$219m)



Local Street Network

6 Lane Expressways

6 Lane Major Arterials

4 Lane Major Arterials

2 Lane Minor Arterials

Major Collectors

Minor Collectors

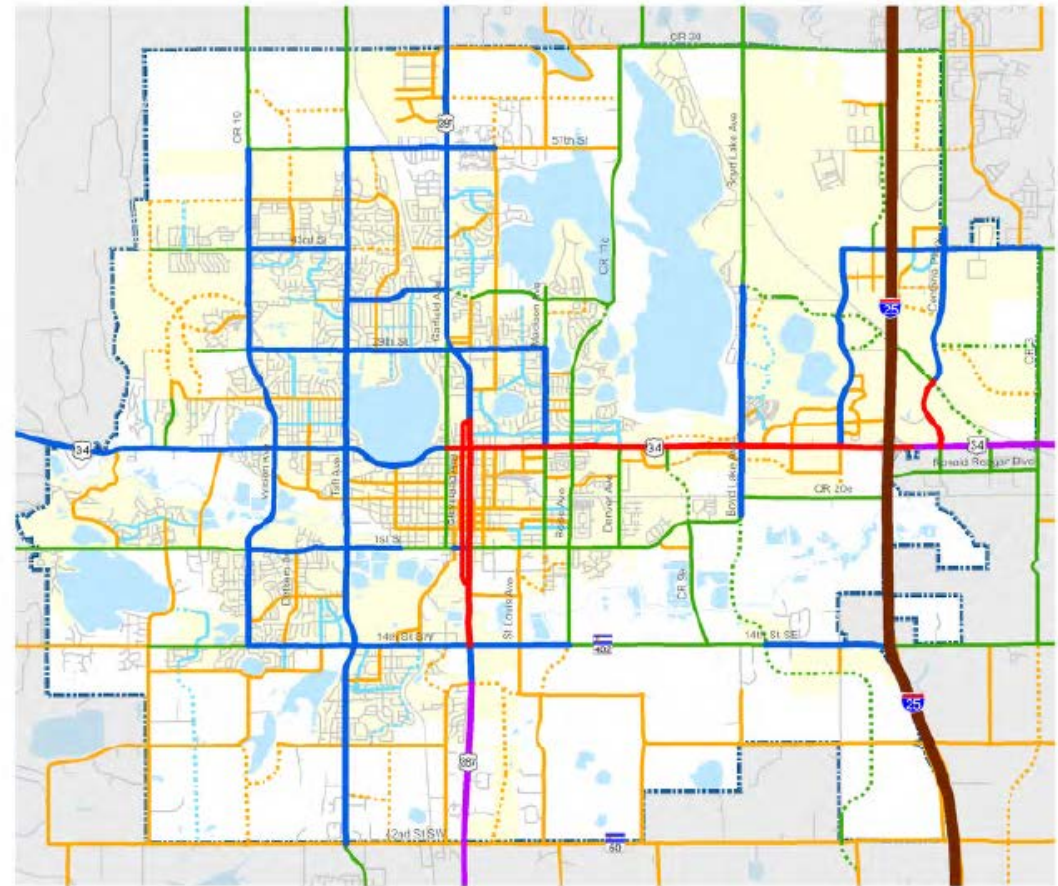
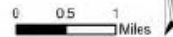


Legend

- City Limits
 - Growth Management Area
 - Railroad
 - Big Thompson River
 - Lakes
- Roadway Designations**
- Freeway 4 lanes
 - Expressway 6 lanes
- Arterials**
- | | | |
|------------------------|------------------------|------------------------|
| Existing | Future | Major Arterial 6 lanes |
| Major Arterial 4 lanes | Major Arterial 4 lanes | Minor Arterial 2 lanes |
- Collectors**
- | | | |
|-----------------|-----------------|-----------------|
| Existing | Future | Major Collector |
| Minor Collector | Minor Collector | |

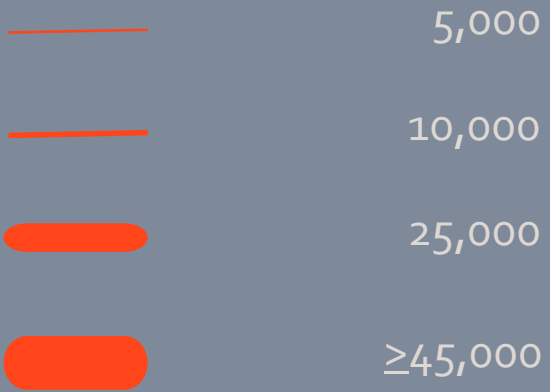
In undeveloped areas, collector streets may be added, deleted, or classifications changed by the Public Works Director to best meet the needs of the areas served by those streets.

Location of future routes and intersections are conceptual only. Exact locations will be determined later by separate study



2035 STREET PLAN

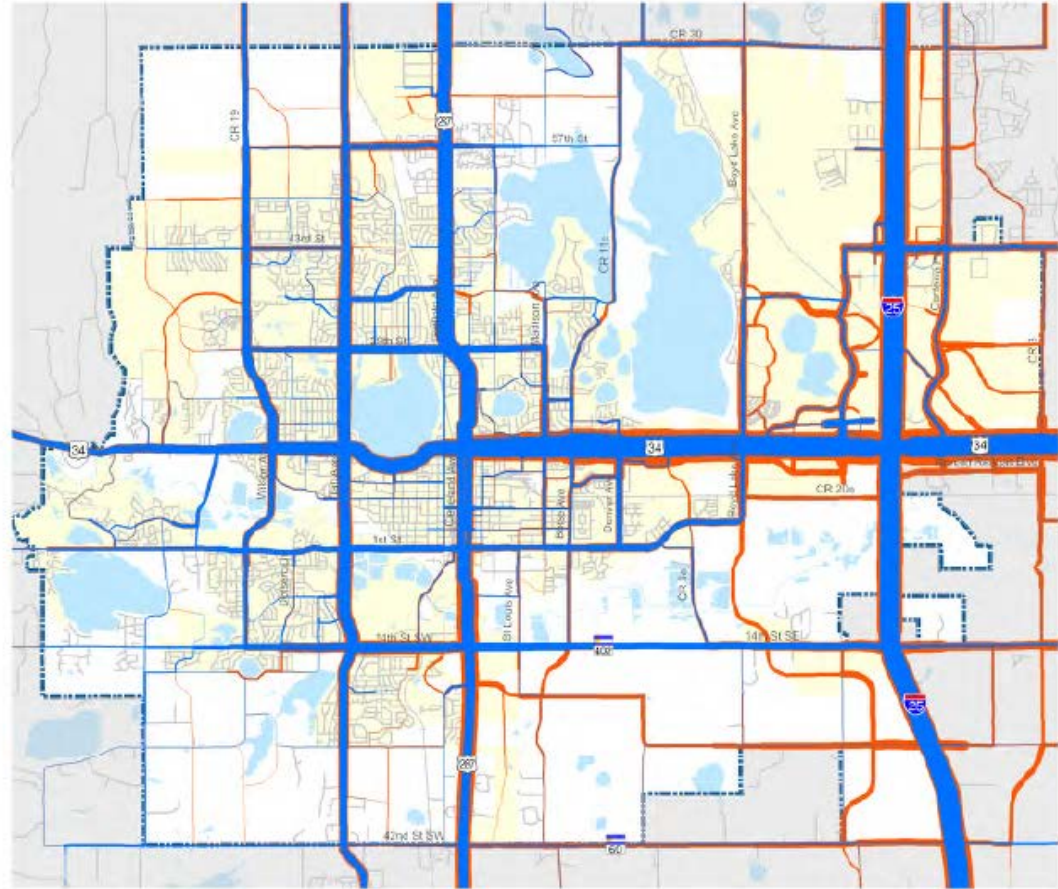
2035 Volumes



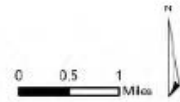
34 is changing faster than we projected and in ways we never predicted.

Johnny Olson, CDOT Region 4 Director

City of Loveland 2035 Transportation Plan



2035 STREET VOLUMES



Capital Projects

US Hwy 34

US34 westbound widening (Denver to Boyd Lake): **\$5m** (2018)

US34/Boyd Lake Intersection and Roundabout at Mountain Lion Drive: **\$3.2m** (2017)

Mountain Lion Drive connection: **Developer fund/build** (2016)

Boyd Lake and 15th Street Roundabout/bridge replacement: **Developer Funded** (YTBD)

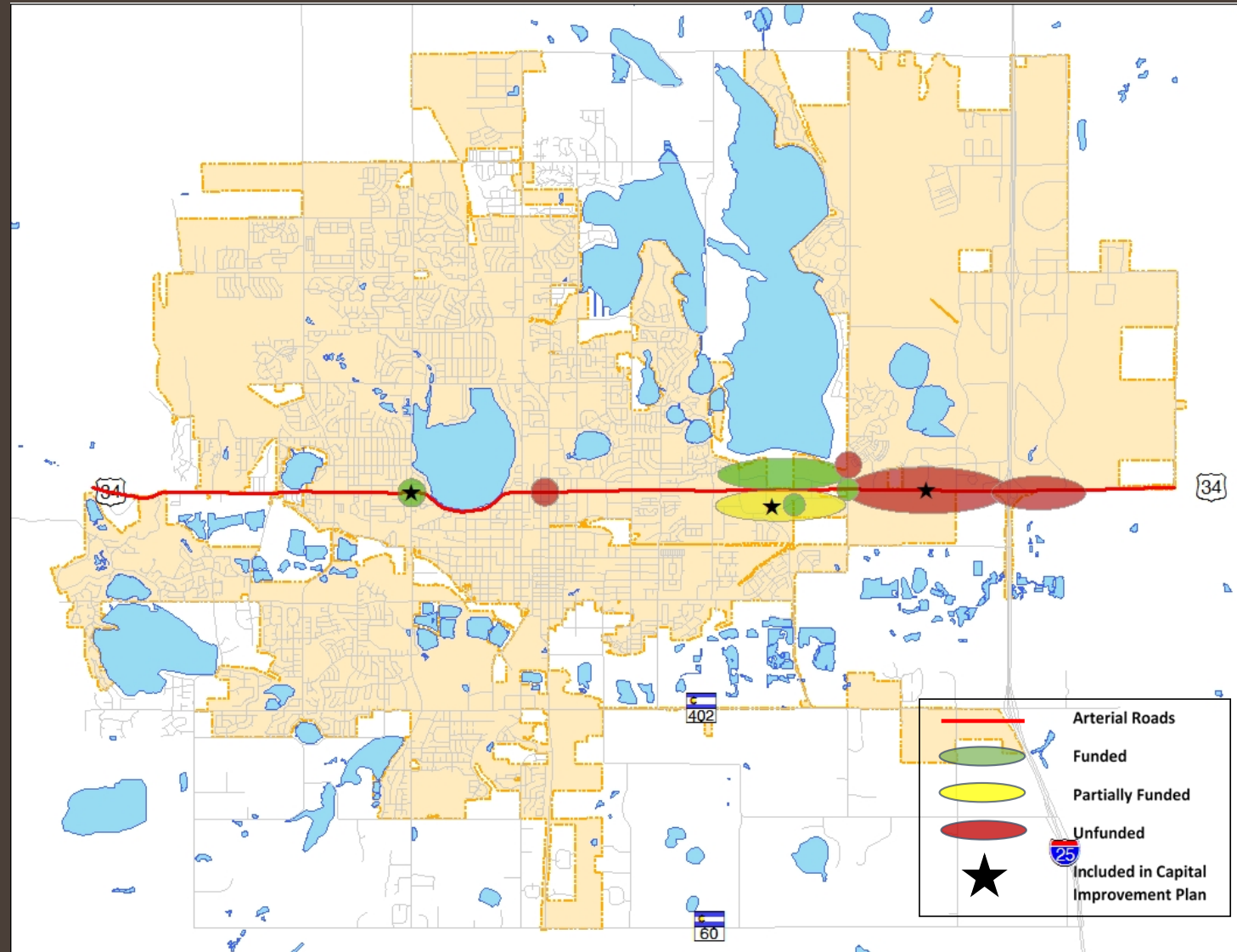
US34 eastbound widening (Denver to Boyd Lake): **\$100,000/\$5.9m** (YTBD)

US34 widening (Boyd Lake to I25): **\$15m** (YTBD)

US34 Improvements (I25 to Centerra Parkway): **\$3.4m** (YTBD)

US34/Taft Avenue Intersection Improvements: **\$1.8m** (2019)

US34/US287 Intersection Improvements: **YTBD** (YTBD)



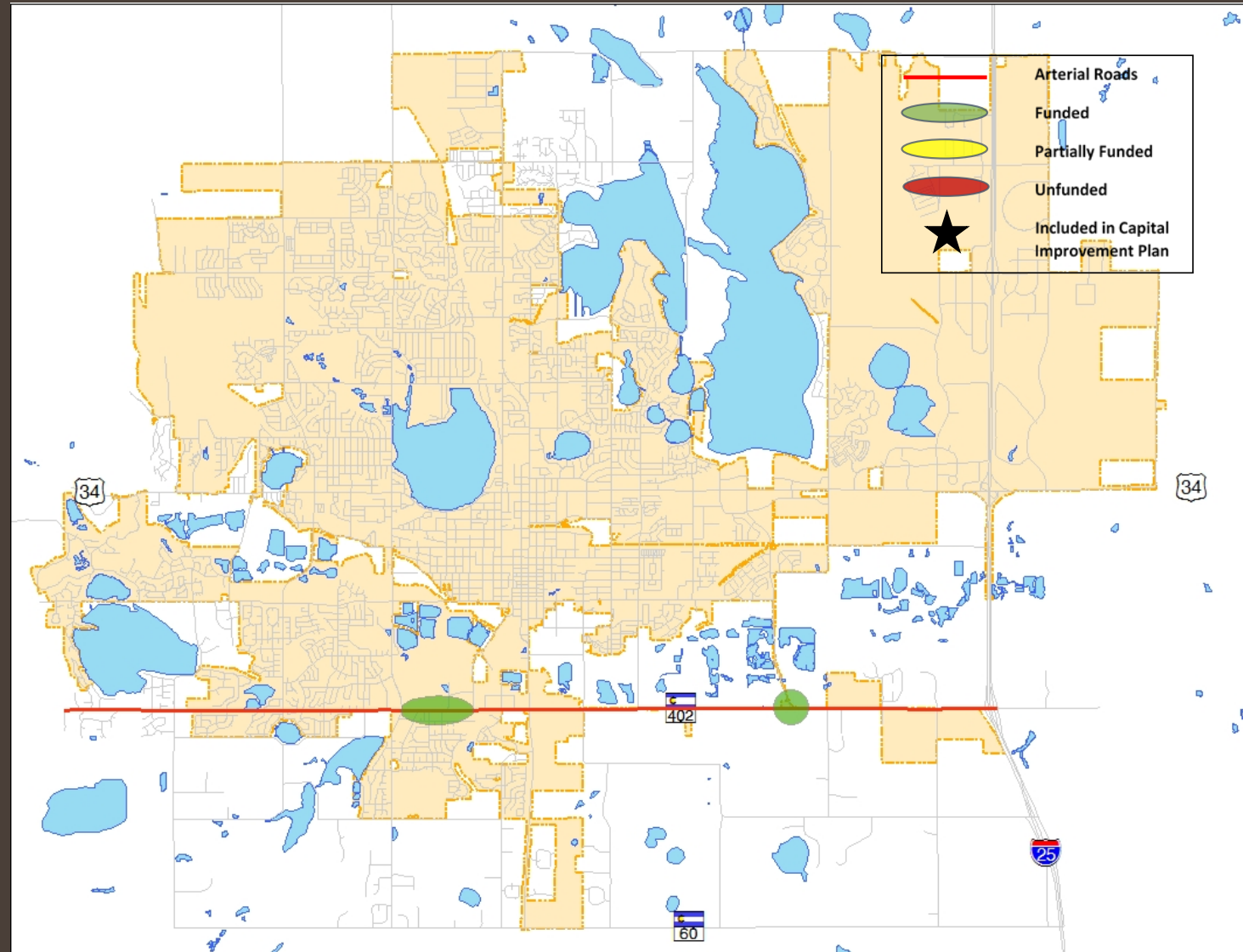
Capital Projects

Hwy 402 (14th Street SW)

CDOT Roadway

14th Street SW resurfacing (Taft Avenue to BNSF railroad): **\$570,000** (2016)

County Rd 9E (Sculptor Dr)/Hwy 402(14th Street SW): **CDOT fund/build** (2016)

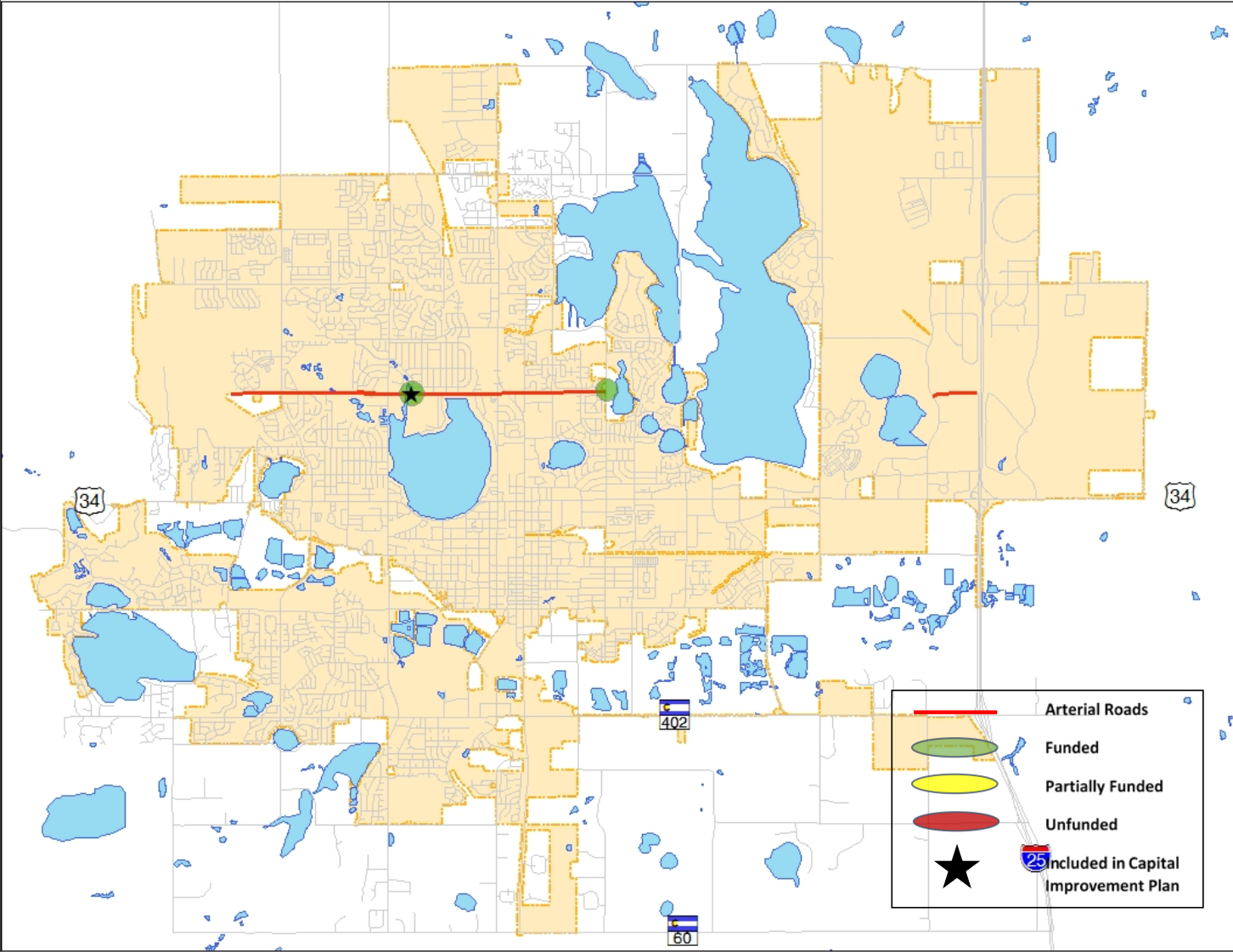


Capital Projects

29th Street

29th Street and Beech Drive, new traffic signal: \$500,000 (2017)

29th Street and Madison intersection spot improvements: \$500,000 (2020)



Capital Projects

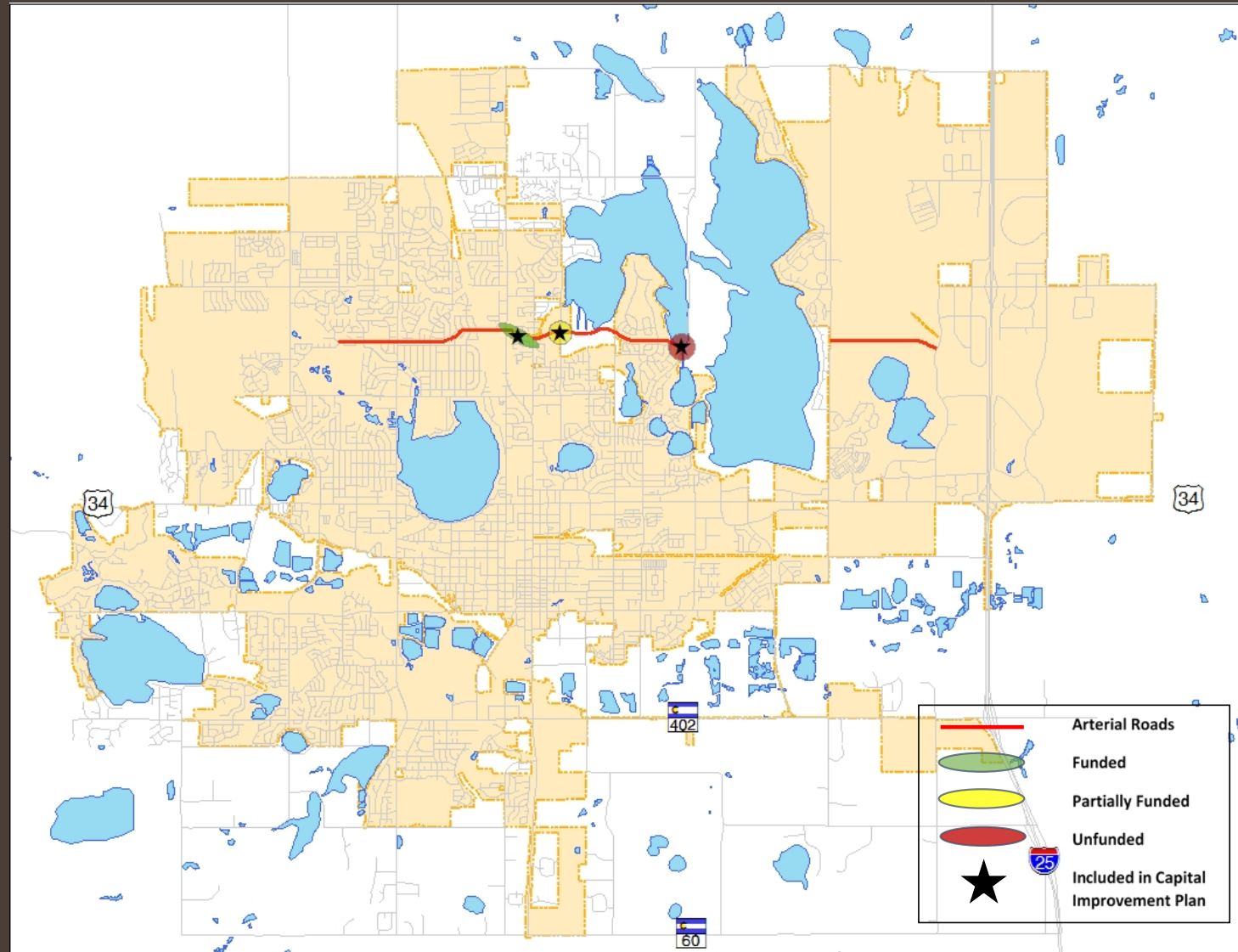
37th Street

37th Street connection (US287 to Lincoln Avenue): **\$2.5m** (2019)*

*Requires a companion stormwater project: **\$1.7m** (2019)

37th Street and Monroe intersection spot improvements: **\$500,000/ \$500,000** (2021)

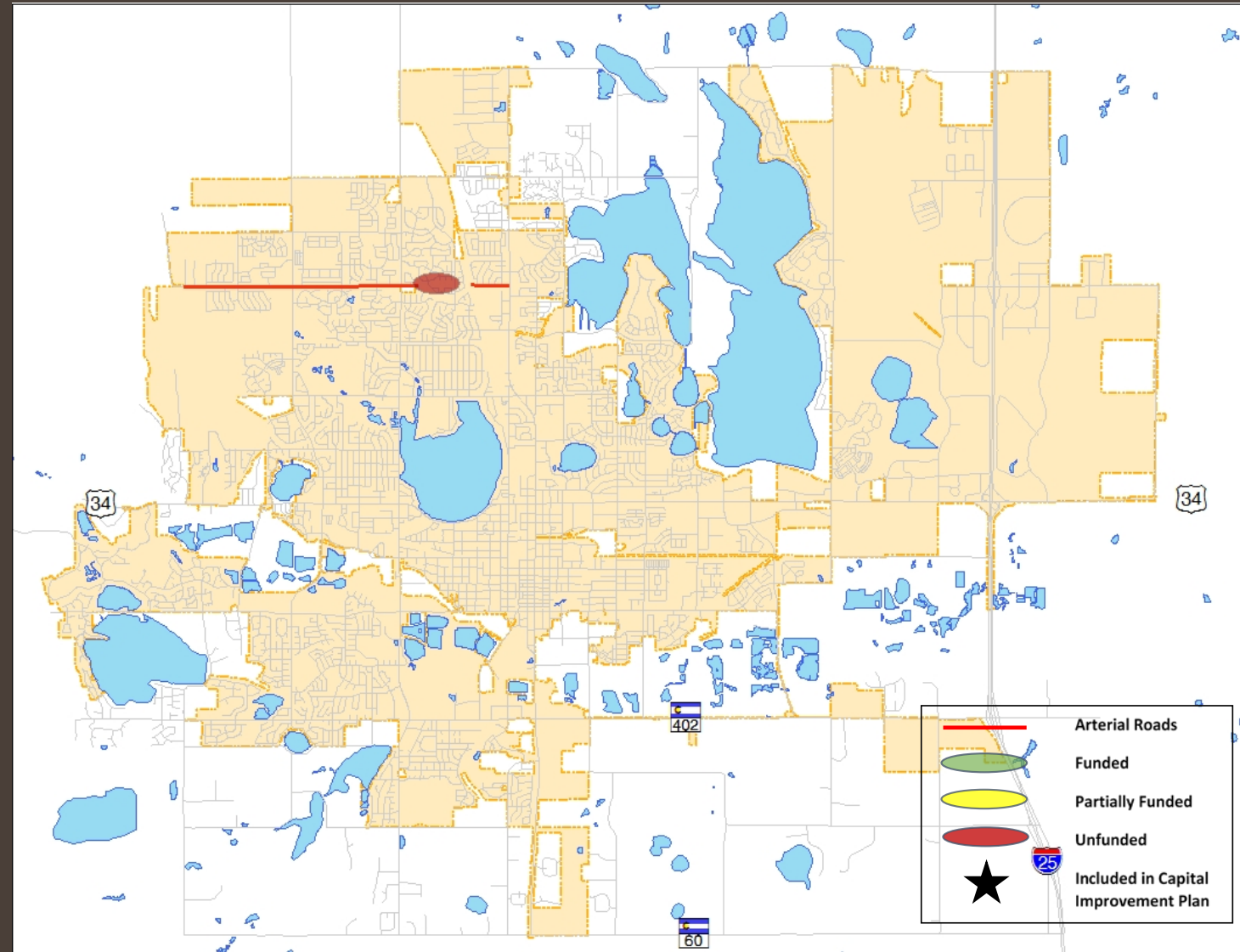
37th Street and Boise Roundabout: **\$2.3m** (2026)



Capital Projects

43rd Street

43rd Street extension from Smith Park Court to Duffield Avenue with culvert: **YTBD** (YTBD)



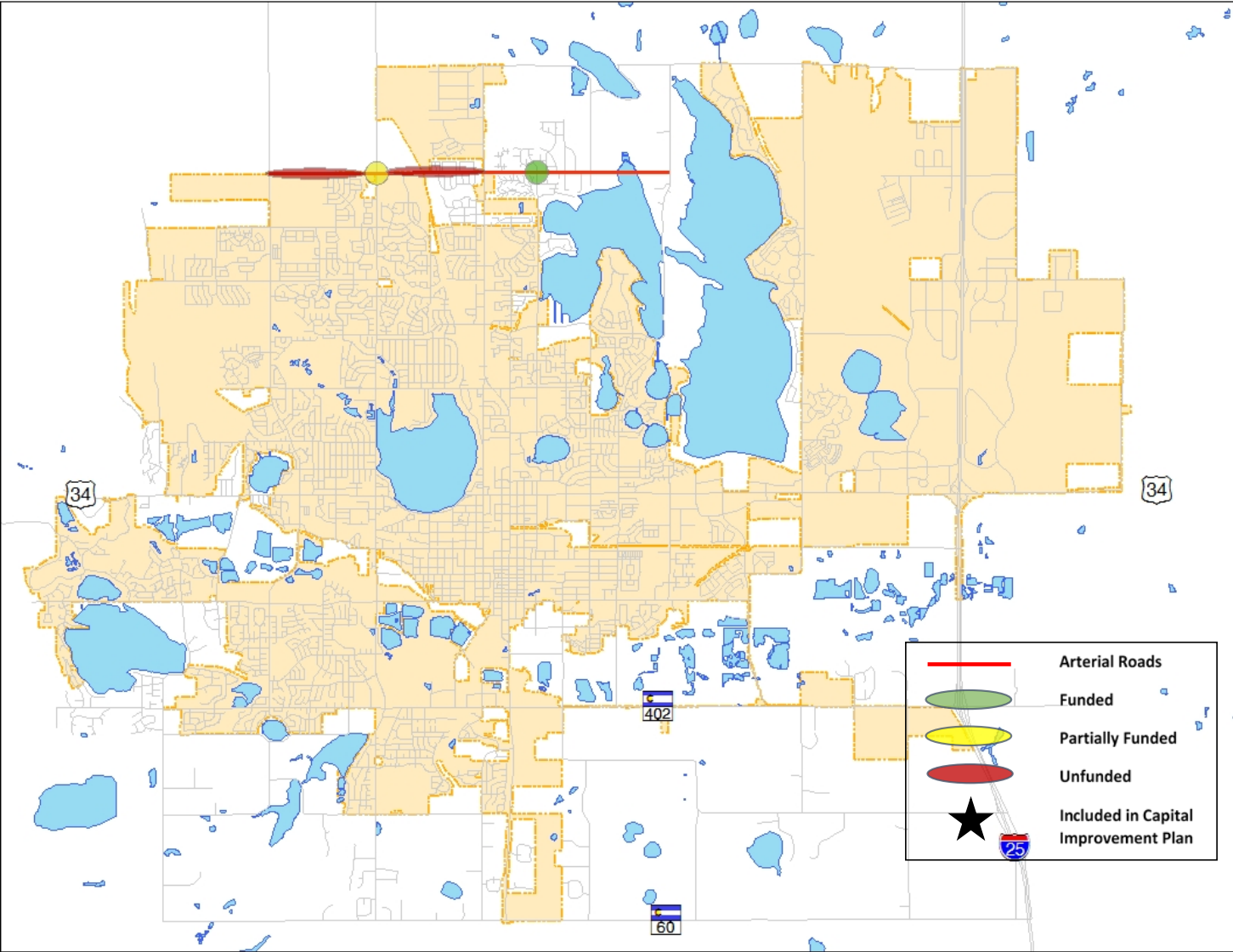
57th Street

57th Street reconstruction (Wilson to Taft Avenue): **\$1.5m** (YTBD)

57th Street/Taft intersection spot improvements: **\$500,000** (2020)

57th Street improvements (Taft to US287): **\$8.9m** (YTBD)

57th Street and Monroe Traffic Signal: **Larimer County Fund/Build** (2016)



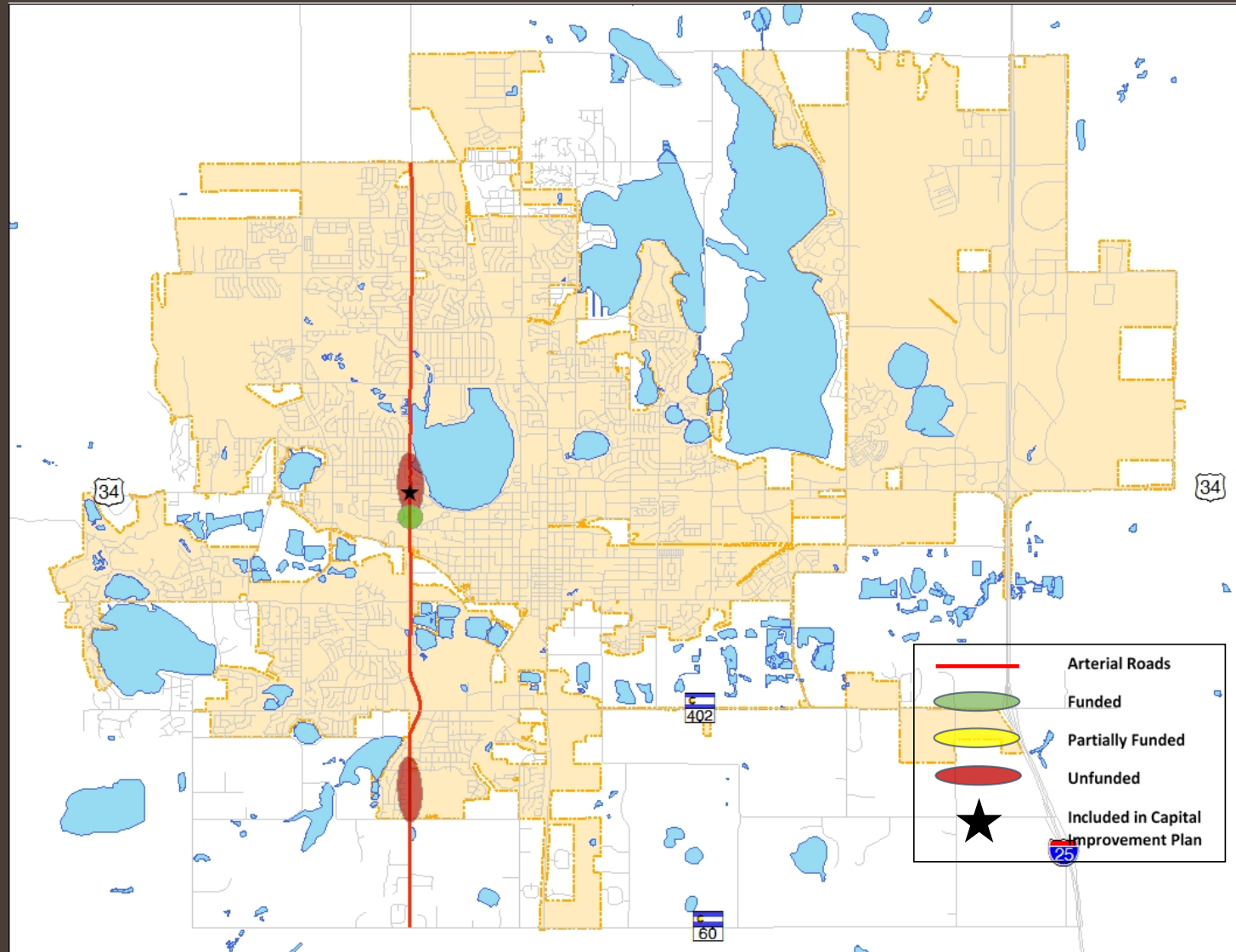
Capital Projects

Taft Avenue

Taft Avenue widening (11th Street to West Shore Dr.): **\$8m** (YTBD)

Taft Avenue Bridge and road widening (8th Street to 11th Street): **\$1.6m** (2016)

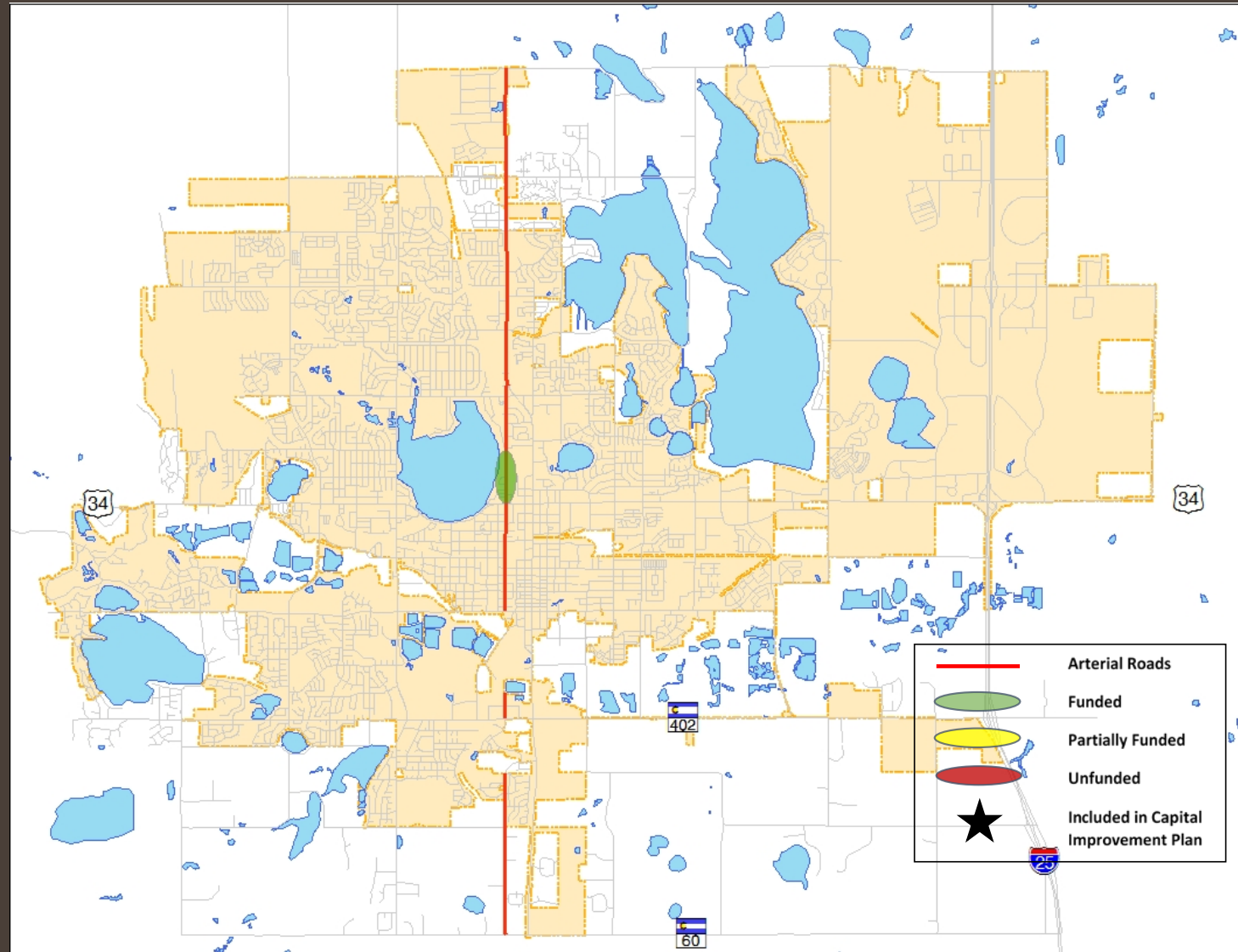
Taft Avenue widening south of 22nd Street SW: – **Developer fund/build** (YTBD)



Capital Projects

Garfield Avenue

Garfield improvements (US34 to BNSF railroad): \$800,000 (2017)

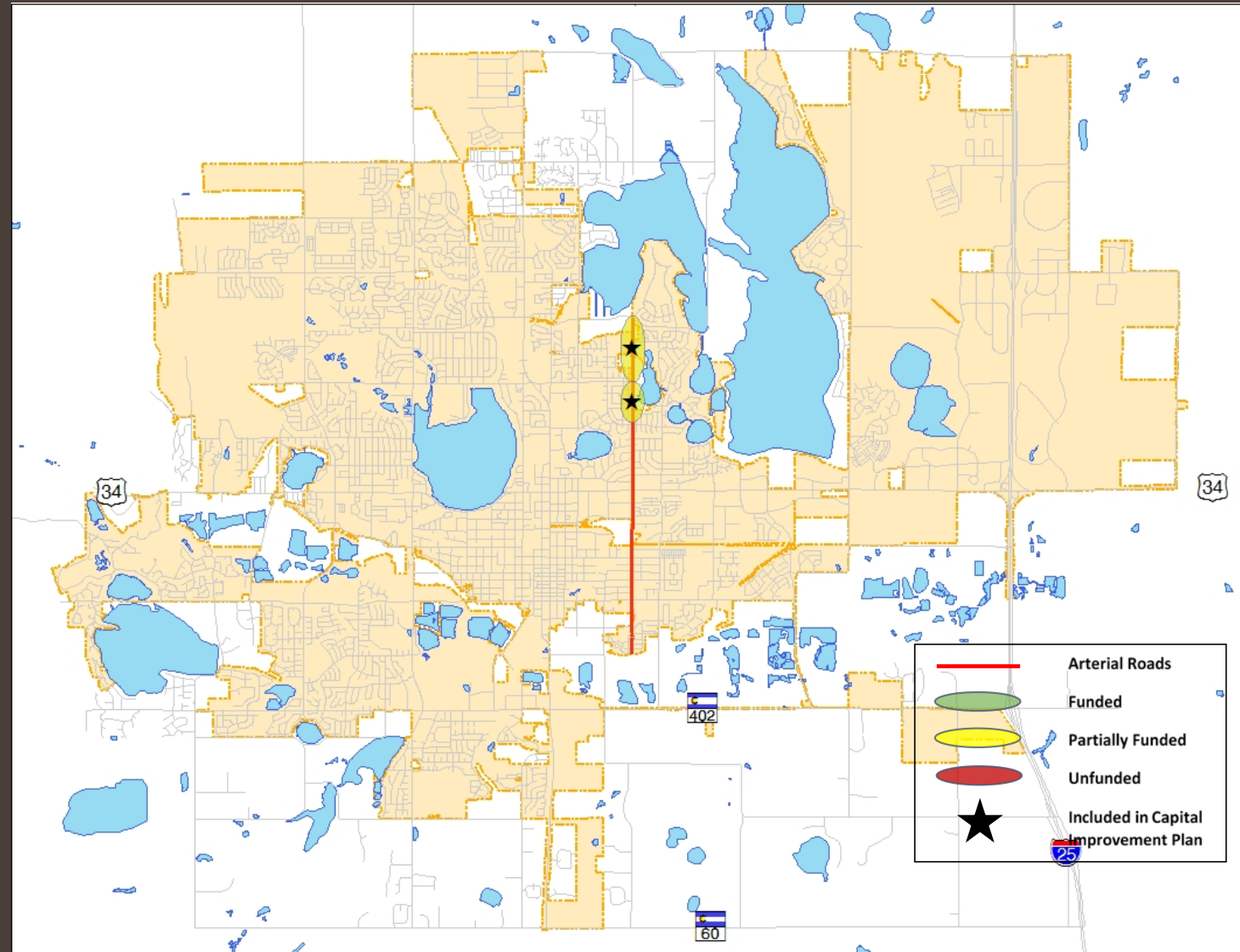


Capital Projects

Madison Avenue

Madison improvements (29th Street to 37th Street): \$500,000/\$1.1m (2021)

Madison improvements (Silverleaf to 29th Street): \$2.14m/\$1.66m (2023)



System Gaps

Duffield extension from Coral Burst Drive to 57th Street: *Developer fund/build \$1m* (YTBD)

11th Street extension from Redwood to Gorom: *Developer fund/build* (YTBD)

Sculptor Drive: *Developer fund/build* (2018)

Kendall Parkway: *Metro District/CDOT fund/build* (YTBD)

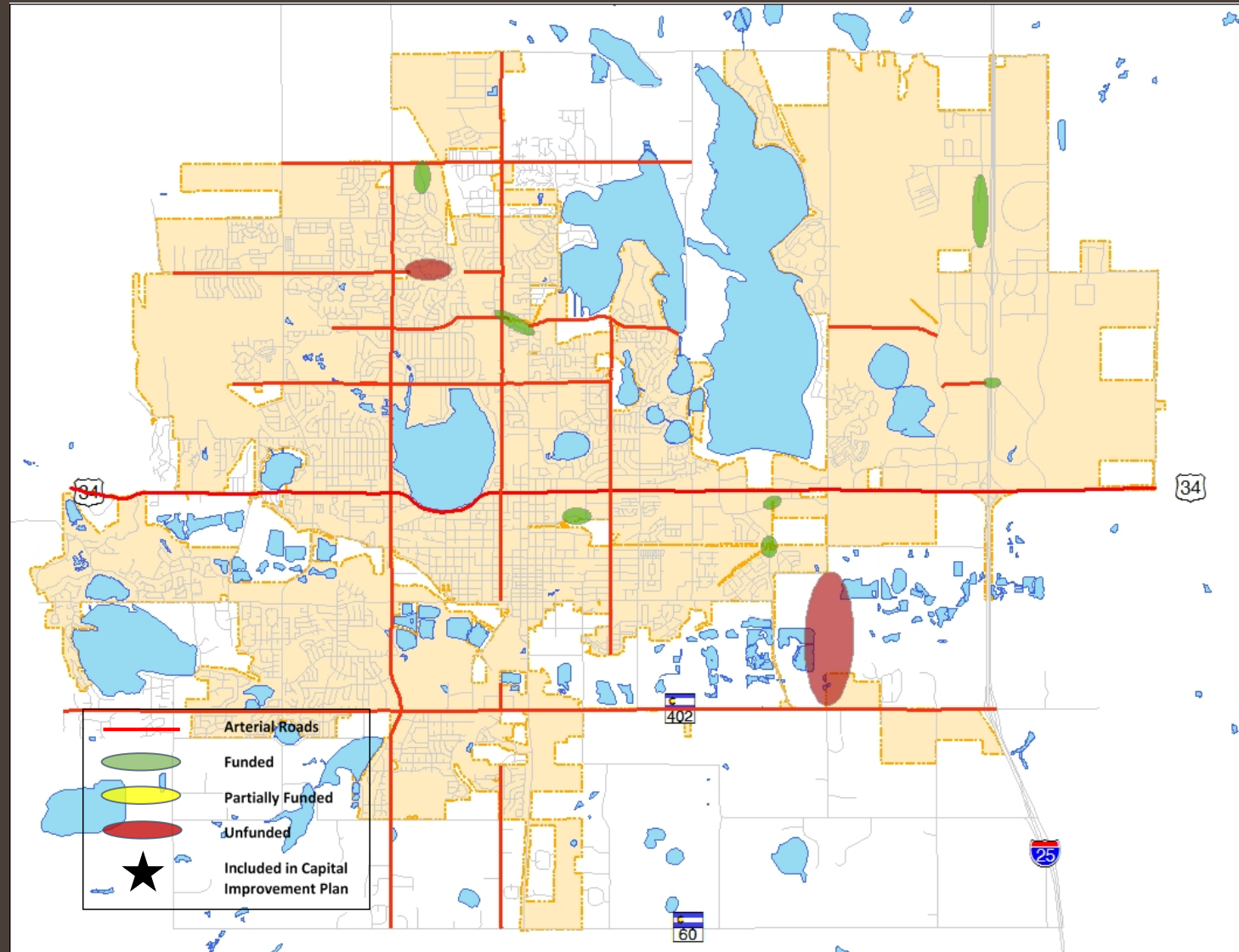
Byrd Drive: *CDOT/Developer/City fund, Developer build* (2016)

Boyd Lake Avenue extension from 5th Street to Hwy402: *YTBD* (YTBD)

43rd Street extension from Smith Park Court to Duffield Avenue with culvert: *YTBD* (YTBD)

37th Street connection from US287 to Lincoln Avenue: *\$2.5m* (2019)

Mountain Lion Drive connection: *Developer fund/build*(2016)

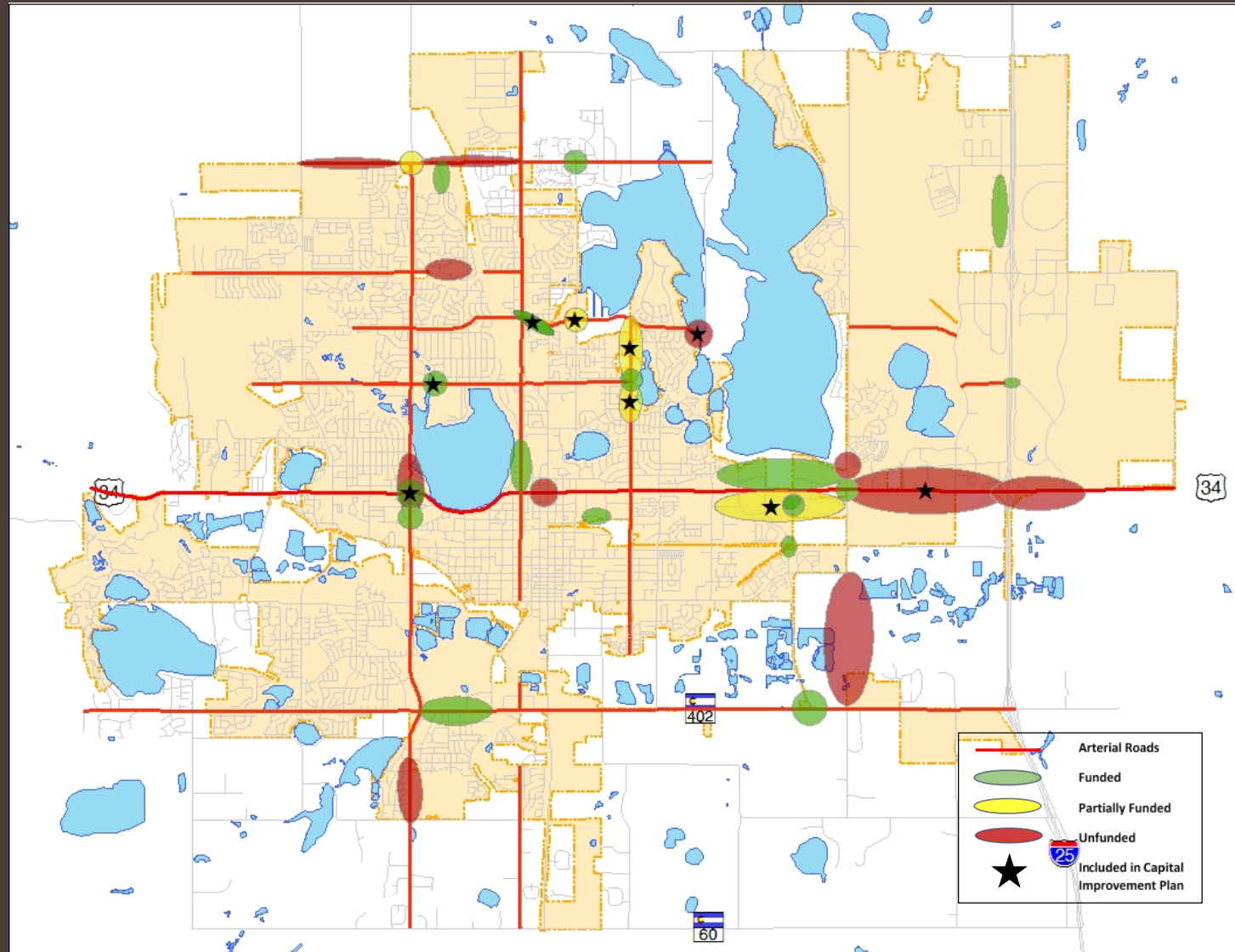


Capital Projects

Unfunded Priorities

1. Eastbound 34 widening (Denver Avenue to Boyd Lake): **\$6m**
2. 57th Street reconstruction (Wilson to Taft Avenue): **\$1.5m**
3. Roundabout at Monroe and 37th Street: **\$500,000/\$500,000**

Council Interest: Boyd Lake and 15th Street Roundabout/bridge replacement: **\$4m-\$5m/Developer Funded (YTBD)**



Bridges

A System of Many Parts

Asset Management

39 National Bridges

48 Minor Structures (estimated)

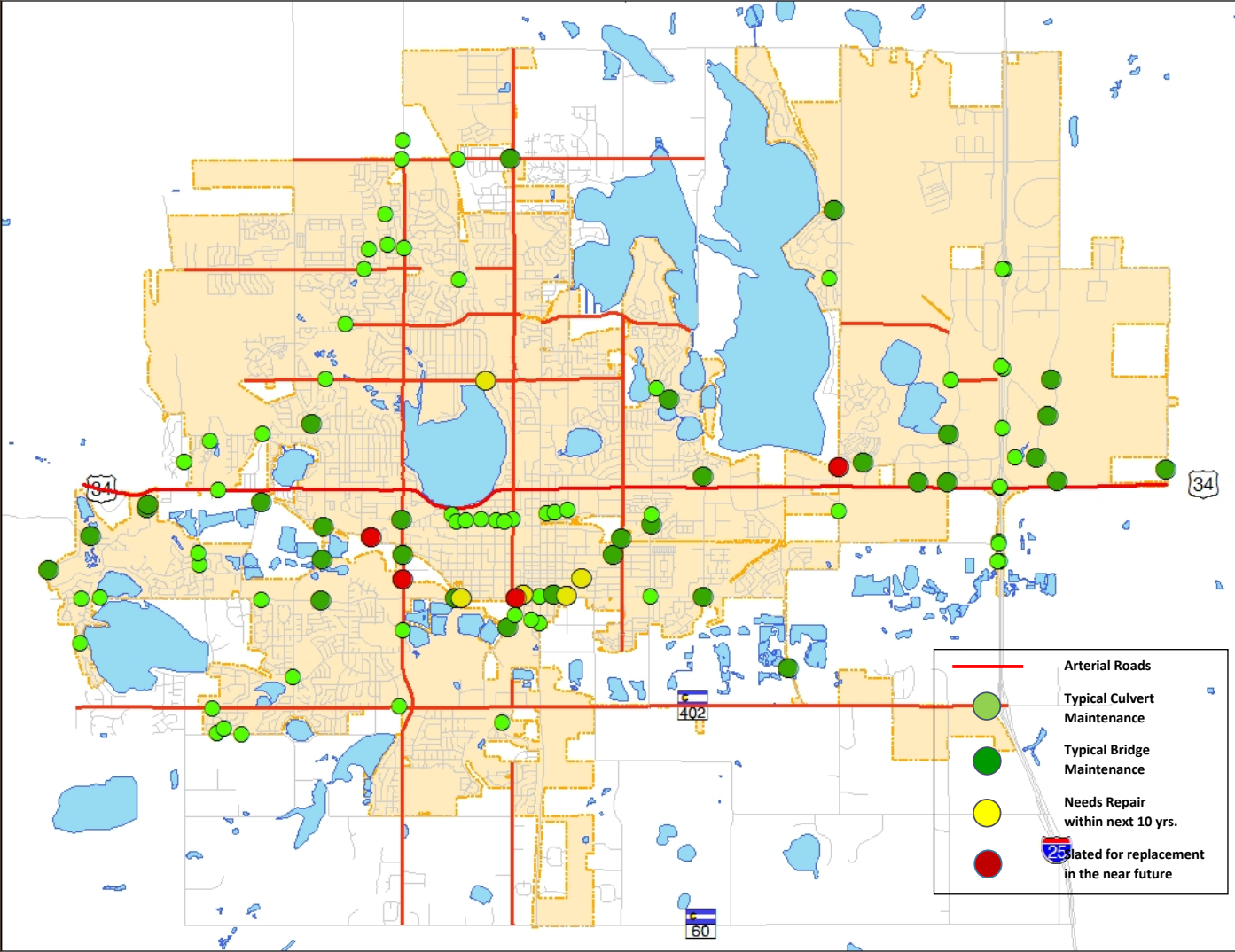
PROJECT NEEDS:

8th Street over Big Barnes – Full Bridge Replacement: **\$1.5m**

1st Street over GLIC – Superstructure Replacement: **\$1m**

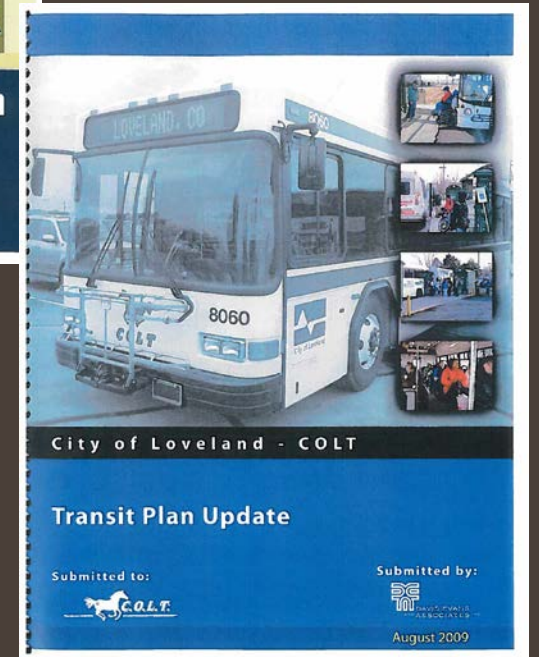
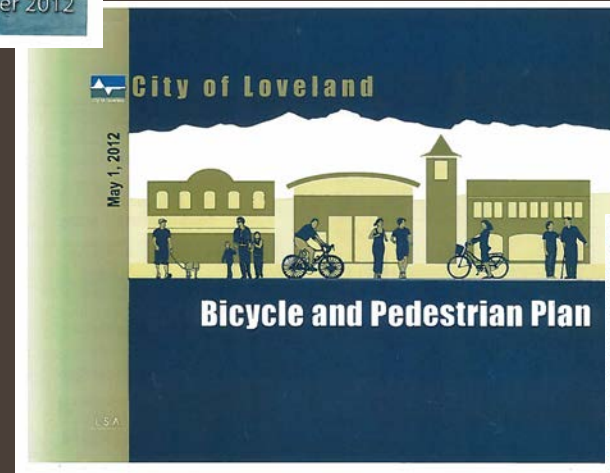
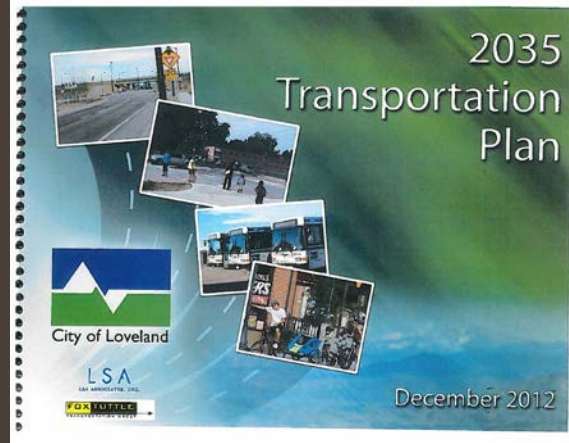
N Taft over Big Thompson – Full Replacement: **\$3m**

Boyd Lake Ave – Full Replacement: **\$1.5m**



Multi-Modal Mobility

- Cars
- Bicycles
- Pedestrians
- Buses
- Commuter Trains



Bike and Ped Plan

Top 300 Projects

Sidewalk on One Side of Street

Bike Projects:

City \$3m to \$6.8m

Other \$3m to \$6.1m

Total \$6m to \$12.9m

Ped Projects:

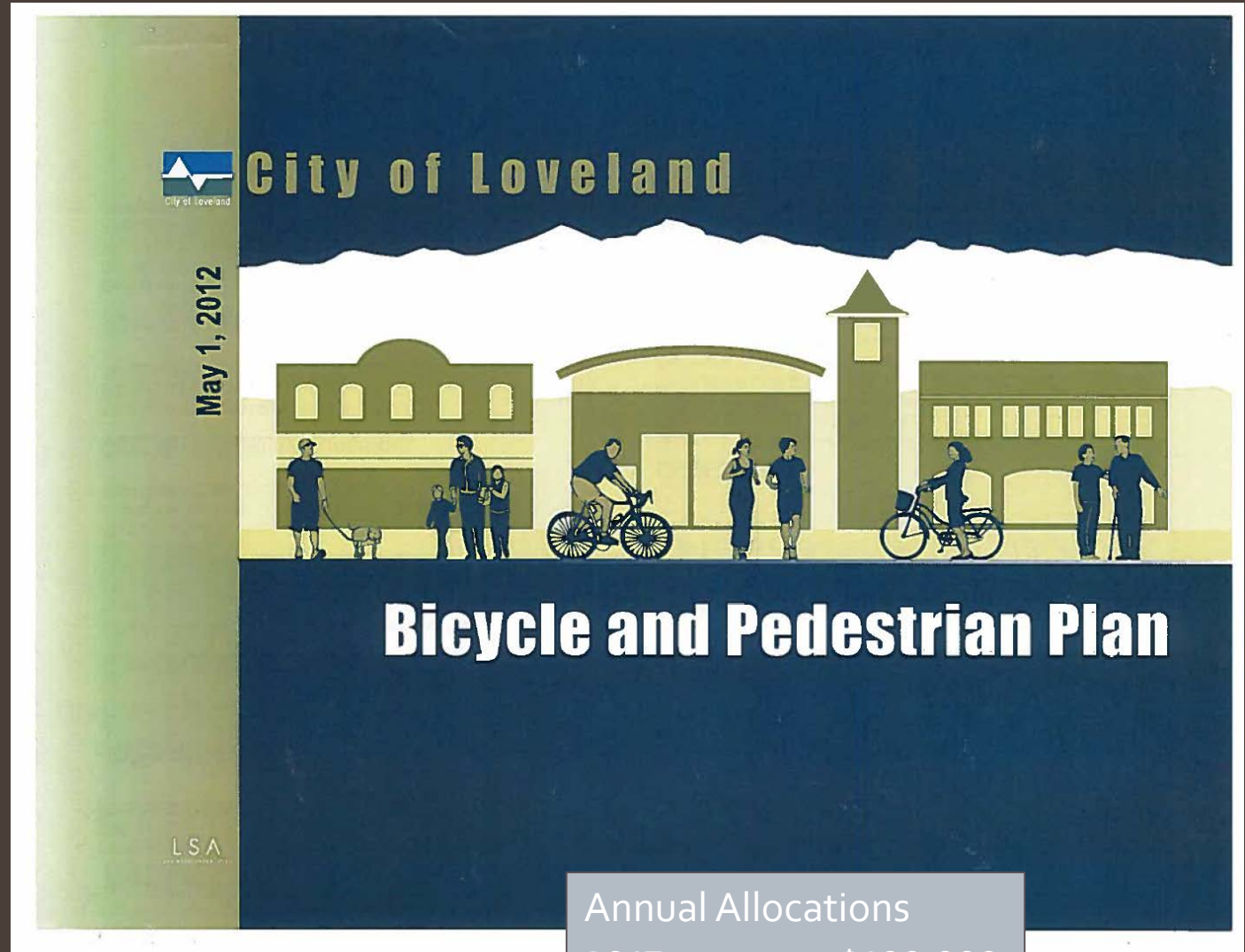
City \$2.7m to \$4.8m

Other \$3.1m to \$6.2m

Total \$5.8m to \$11m

TOTAL BIKE and PED:

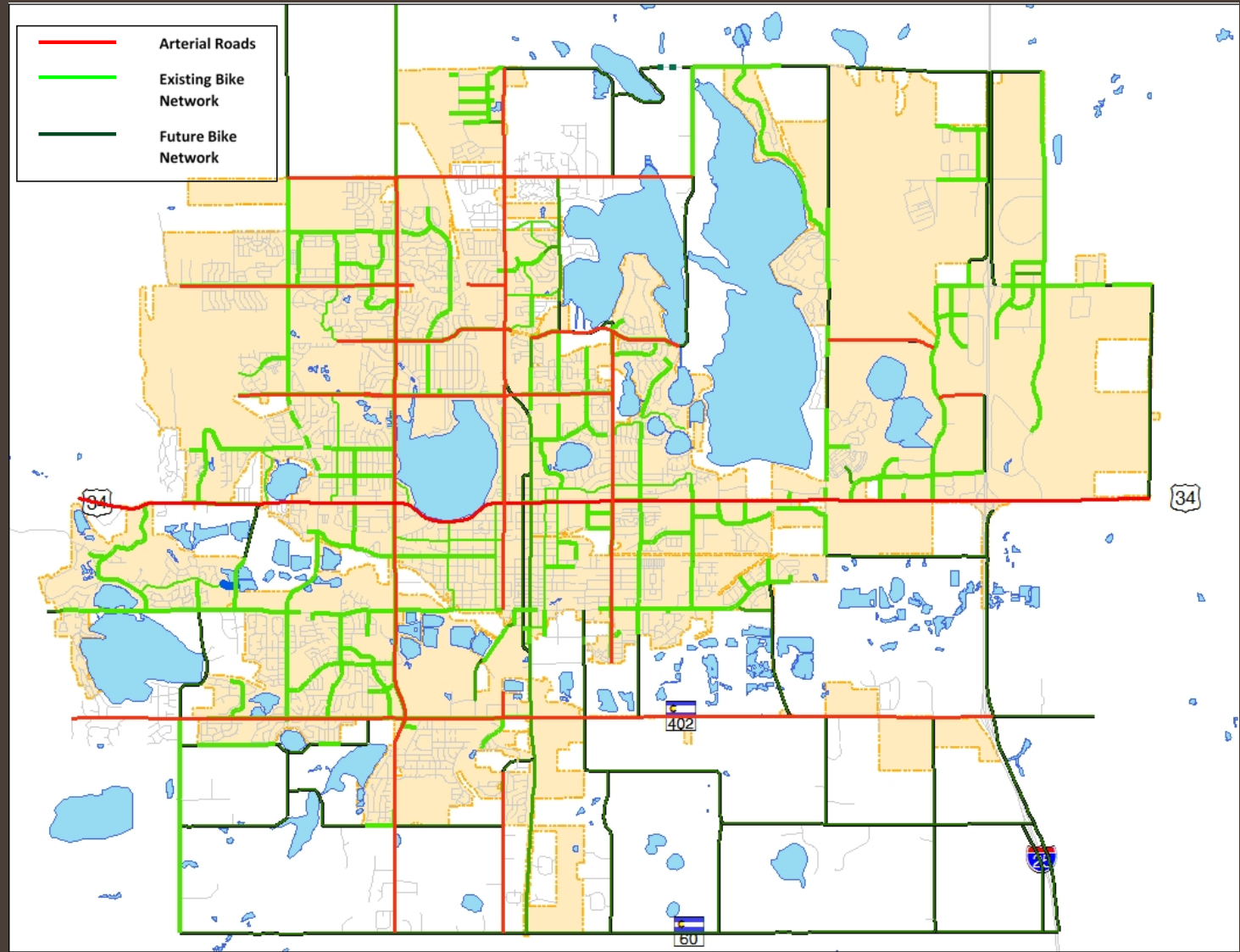
\$12.8m to \$25.9m



Annual Allocations	
2017:	\$100,000
2018:	\$50,000

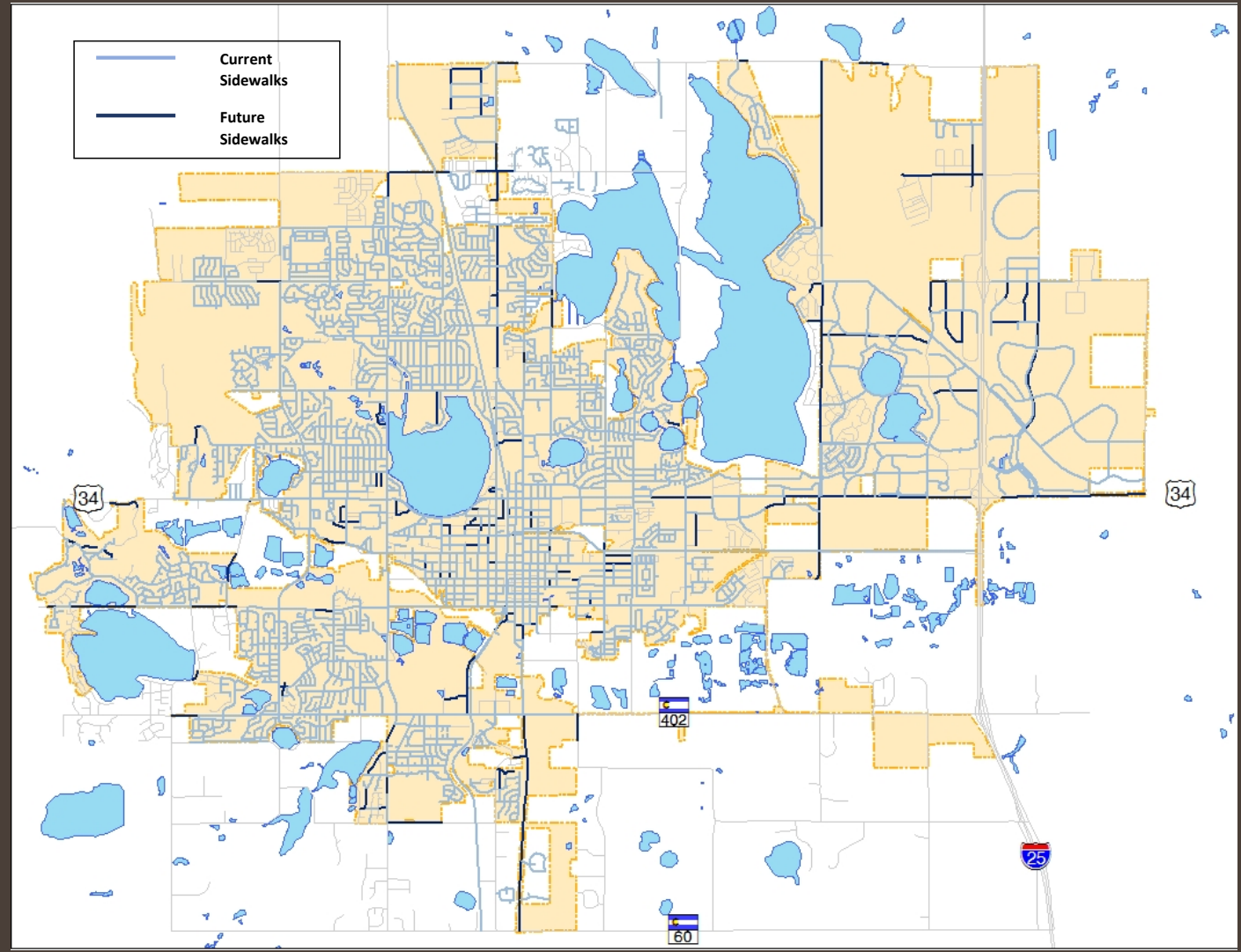
Bikes

Future Bicycle Network



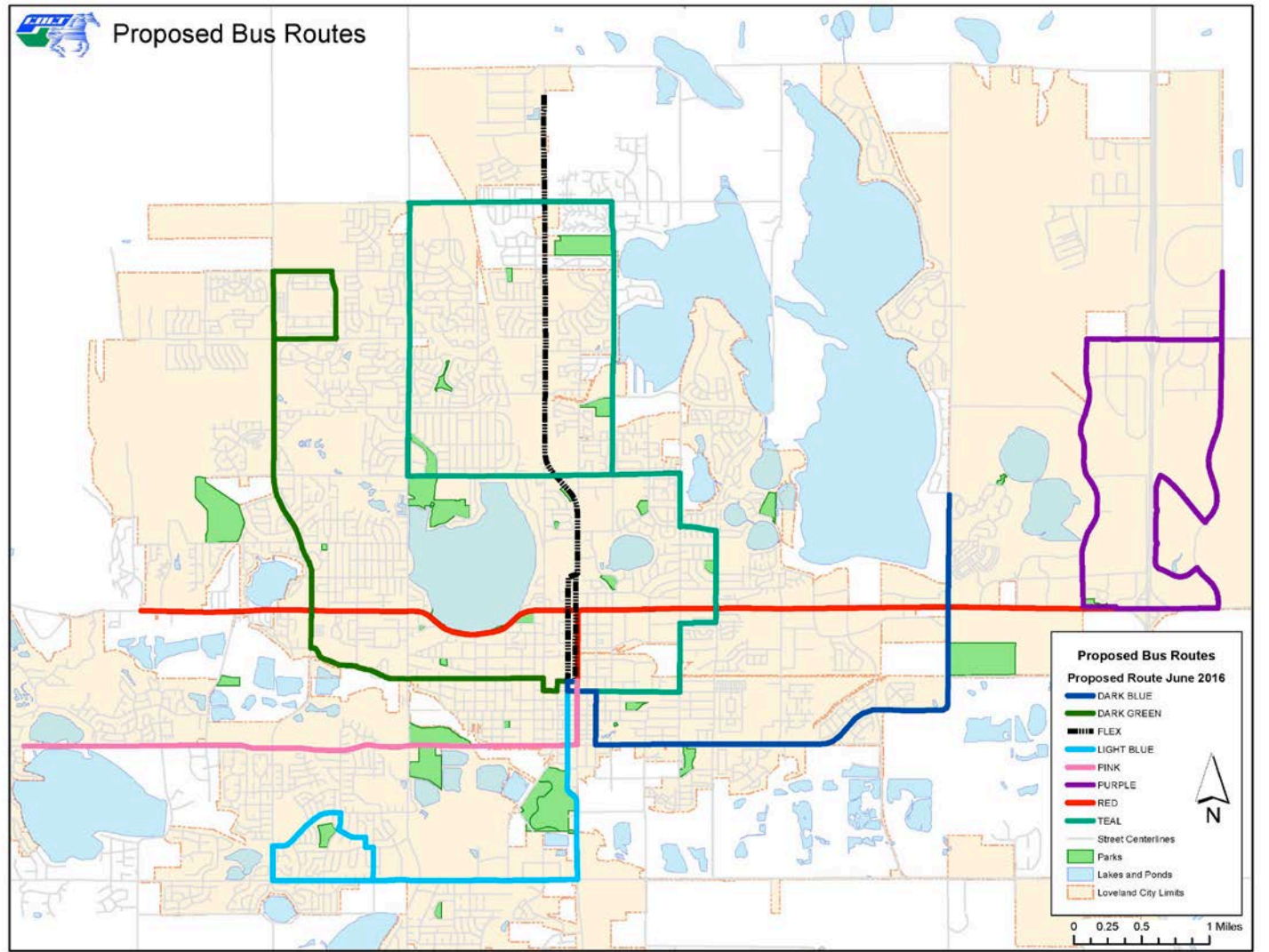
Peds

Future Pedestrian Network



Buses

Future
Transit
Network?



Pavement

Roadway Maintenance

Pavement Management System

340 centerline miles

7.1m square yards of surface

+180,000 sq yards/year

\$772m value

72PCI

\$4.7m annual budget

PCI drops to 67 in 10 years

\$5.8m to maintain 72PCI



PCI 95
¢50 to ¢65/sq ft



PCI 75



PCI 68
¢35 to \$4.25 /sq ft



PCI 40
\$4 to \$5/sq ft



PCI 21
\$8.50 to \$15/sq ft

Open City Hall #1

Traffic Congestion

- From the Public:
 - Top Priorities
 - Alternate Routes
 - New Issues
- Then:
 - Peer Review
 - Traffic Operations
 - Signal Timing
 - Consultant Support
 - Real-time Signal Timing Adjustment
 - Future Capital Projects

Open City Hall #2

Traffic Safety

- Traffic Safety with LPD
 - Top 3 Priorities
 - Major Intersection Analysis
- Then:
 - Customized Plan
 - Each Priority Topic
 - Each Intersection
 - 3 E's
 - Engineering
 - Enforcement
 - Education



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WEEK

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Conclusion

Questions and Discussion